

Kingborough



COUNCIL MEETING AGENDA

NOTICE is hereby given that an Ordinary meeting of the Kingborough Council will be held in the Kingborough Civic Centre, 15 Channel Highway, Kingston on
Monday, 20 January 2025 at 5.30pm

Kingborough Councillors 2022 - 2026



Mayor
Councillor Paula Wriedt



Deputy Mayor
Councillor Clare Glade-Wright



Councillor Aldo Antolli



Councillor David Bain



Councillor Gideon Cordover



Councillor Kaspar Deane



Councillor Flora Fox



Councillor Amanda Midgley



Councillor Mark Richardson



Councillor Christian Street

QUALIFIED PERSONS

In accordance with Section 65 of the *Local Government Act 1993*, I confirm that the reports contained in Council Meeting Agenda No. 1 to be held on Monday, 20 January 2025 contain advice, information and recommendations given by a person who has the qualifications or experience necessary to give such advice, information or recommendations.



Dave Stewart
CHIEF EXECUTIVE OFFICER
being the General Manager as appointed by the
Kingborough Council pursuant to section 61 of the
Local Government Act 1993 (TAS)

Tuesday, 14 January 2025

Public Copy

Table of Contents

Item		Page No.
Open Session		
1	Audio Recording	1
2	Acknowledgement of Traditional Custodians	1
3	Attendees	1
4	Apologies	1
5	Confirmation of Minutes	1
6	Workshops held since Last Council Meeting	1
7	Declarations of Interest	2
8	Transfer of Agenda Items	2
9	Questions Without Notice from the Public	2
10	Questions on Notice from the Public	2
	10.1 LPS Exhibition Process	2
	10.2 Public Display of Christmas Decorations by Kingborough Council	2
	10.3 Planning Department Staffing Levels	3
11	Questions Without Notice from Councillors	4
12	Questions on Notice from Councillors	4
13	Officers Reports to Planning Authority	5
	13.1 Proposed Amendment to the Kingborough Interim Planning Scheme 2015 to Rezone Land at 26 Crescent Drive (CT146336/1, CT146336/2 and CT146336/3) and 21 Gemalla Road (CT187452/1), Margate	5
	13.2 DA-2024-239 - Development Application for Seven (7) Warehouses, Two (2) Showrooms and Associated Works at 'Kingston Town', 37-59 Maranoa Road, Kingston and Adjoining Council Road Reserve	39
14	Petitions still being Actioned	116
15	Petitions Received in Last Period	116
16	Officers Reports to Council	116
	16.1 Cash-in-Lieu of Parking for John Street Medical Centre	116
	16.2 Kingborough Community Safety Committee Review	120
17	Notices of Motion	132
	17.1 Infrastructure Projects	132
18	Confirmation of Items to be Dealt with in Closed Session	133

Table of Contents

Item		Page No.
Appendices		
A	Chief Executive Officer's Activities 4 November 2024 to 20 December 2024	136
B	Infrastructure Works Report September 2024 to December 2024	138
C	Audit Panel Minutes 6 December 2024	150
D	Kingborough Bicycle Advisory Committee Minutes 13 Dec 2024	155
E	Complaints Lodged with Council 1 October 2024 to 31 December 2024	168
F	Councillor Attendance at Meetings and Workshops 1 October 2024 to 31 December 2024	170
G	Councillor Allowances & Expenses 1 July 2024 to 31 December 2024	171
H	Donations Table 1 July 2024 to 31 December 2024	172

Public Copy

GUIDELINES FOR PUBLIC QUESTIONS

Section 31 of the Local Government (Meeting Procedures) Regulations 2015

Questions from the public may either be submitted to the General Manager in writing or asked verbally at an Ordinary Council meeting. Any question asked must only relate to the activities of Council [Section 31(2)(b)].

This guideline is provided to assist the public with the requirements of Public Question Time as set out in the *Local Government (Meeting Procedures) Regulations 2015* as well as determinations made by Council. You are reminded that the public question forum is designed to accommodate questions only and neither the questions nor answers will be debated.

Questions on Notice

Written questions on notice must be received at least seven (7) days before an Ordinary Council meeting [Section 31(1)] and must be clearly headed 'Question/s on Notice'. The period of 7 days includes Saturdays, Sundays and statutory holidays but does not include the day on which notice is given or the day of the Ordinary Council meeting [Section 31(8)].

Questions Without Notice

The Chairperson of an Ordinary Council meeting must ensure that, if required, at least 15 minutes is made available for public questions without notice [Section 31(3)]. A question without notice must not relate to any matter that is listed on the agenda for that meeting.

A question by any member of the public and an answer to that question is not to be debated at the meeting [Section 31(4)]. If a response to a question cannot be provided at the meeting, the question will be taken on notice and will be included in the following Ordinary Council meeting agenda, or as soon as practicable, together with the response to that question.

There is to be no discussion, preamble or embellishment of any question asked without notice, and the Chairperson may require that a member of the public immediately put the question.

The Chairperson can determine whether a question without notice will not be accepted but must provide reasons for refusing to accept the said question [Section 31 (6)]. The Chairperson may require a question without notice to be put on notice and in writing.

The Chairperson may rule a question inappropriate, and thus inadmissible if in his or her opinion it has already been asked, is unclear, irrelevant, offensive or relates to any matter which would normally be considered in Closed Session. The Chairperson may require that a member of the public immediately put the question.

AGENDA of an Ordinary Meeting of Council
Kingborough Civic Centre, 15 Channel Highway, Kingston
Monday, 20 January 2025 at 5.30pm

1 AUDIO RECORDING

The Chairperson will declare the meeting open, welcome all in attendance and advise that Council meetings are recorded and made publicly available on its website. In accordance with Council’s policy the Chairperson will request confirmation that the audio recording has commenced.

2 ACKNOWLEDGEMENT OF TRADITIONAL CUSTODIANS

The Chairperson will acknowledge the traditional custodians of this land, pay respects to elders past and present, and acknowledge today’s Tasmanian Aboriginal community.

3 ATTENDEES

Councillors:

- Mayor Councillor P Wriedt
- Deputy Mayor Councillor C Glade-Wright
- Councillor A Antolli
- Councillor G Cordover
- Councillor K Deane
- Councillor F Fox
- Councillor A Midgley
- Councillor M Richardson
- Councillor C Street



4 APOLOGIES

Councillor D Bain

5 CONFIRMATION OF MINUTES

RECOMMENDATION

That the Minutes of the open session of the Council Meeting No. 23 held on 16 December 2024 be confirmed as a true record.

6 WORKSHOPS HELD SINCE LAST COUNCIL MEETING

Date	Topic	Detail
6 January	CEO’s Priorities	The CEO presented organisational insights and priorities for the coming year.

7 DECLARATIONS OF INTEREST

In accordance with Regulation 8 of the *Local Government (Meeting Procedures) Regulations 2015* and Council's adopted Code of Conduct, the Mayor requests Councillors to indicate whether they have, or are likely to have, a pecuniary interest (any pecuniary benefits or pecuniary detriment) or conflict of interest in any item on the Agenda.

8 TRANSFER OF AGENDA ITEMS

Are there any items, which the meeting believes, should be transferred from this agenda to the closed agenda or from the closed agenda to the open agenda, in accordance with the procedures allowed under Section 15 of the *Local Government (Meeting Procedures) Regulations 2015*.

9 QUESTIONS WITHOUT NOTICE FROM THE PUBLIC

10 QUESTIONS ON NOTICE FROM THE PUBLIC

Council has determined that questions on notice or questions taken on notice from a previous meeting should not contain lengthy preambles or embellishments and should consist of a question only. To this end, Council reserves the right to edit questions for brevity so as to table the question only, with some context if need be, for clarity.

10.1 LPS Exhibition Process

At the Council meeting on 16 December 2024, **Mr Caleb Elcock** asked the following question without notice to the Chief Executive Officer, with a response that the question would be taken on notice:

You may or may not be aware that my brother and I, so that's Caleb and Nathaniel, submitted around 818 individual representations as we see it. Will that include commentary and assessment on each land title as they have come in as?

Officer's Response:

All properties mentioned/raised in representations will be considered and included in the 35F report that will go to Council before proceeding to the Tasmanian Planning Commission.

Tasha Tyler-Moore, Manager Development Services

10.2 Public Display of Christmas Decorations by Kingborough Council

Mr Paul Mitchell submitted the following question on notice:

- 1. Why did Kingborough Council not display any Christmas decorations in public spaces in 2024?*
- 2. Who was responsible for the decision not to display Christmas decorations in public spaces in 2024, and when was this decision made?*
- 3. What are Council's plans regarding the public display of Christmas decorations in 2025?*

4. *What other causes or events has the Council publicly displayed decorations for in the past two years (2023-2024)? (Please provide details of the occasions, types of decorations displayed, locations and cost)*
5. *What causes or events does the Council plan to publicly display decorations for in 2025? (Please provide details of planned occasions, types of decorations, proposed locations and cost)*

Officer's Response:

1. Currently Council does not own any Christmas decorations.
2. There was no formal decision to not display decorations in 2024.
3. Council currently does not have any plans regarding the display of Christmas decorations for 2025, however Council will investigate options for 2025 and future years having regard to available resources and organisational priorities.
4. There have been no other causes or events that Council have displayed decorations for in 2023 or 2024. There are instances where coloured lighting has been displayed at the Civic Centre and/or the Community Hub for causes or events, for example yellow lighting for Road Safety Week. Council has a *Civic Centre Flagpole Policy* which contains an endorsed flag flying schedule. The policy sets out the process for flying flags on the Council's fourth flagpole which is installed at Council's Civic Centre in Kingston. Community groups and members may apply to have a flag flown at the Civic Centre, in addition to the Australian Flag, the Tasmanian Flag and the Aboriginal Flag.
5. There are no formal plans for displaying decorations in 2025.

Deleeze Chetcuti, Director Environment, Development & Community Services

10.3 Planning Department Staffing Levels

Mr Jarryd Knightley submitted the following question on notice:

1. *Can council please provide the level of staffing within the Planning Department over the last 5 years?*
2. *Can council please provide the total of hours worked each year by council planning staff for each of the previous 5 years?*
3. *Who is paying for the transition to the new State Planning scheme, this is inclusive of ALL hours worked by council staff over the last 10 years and all supporting reports by third parties etc?*
4. *Does council track all costs/hours associated with the transition to the new State planning scheme?*

Officer's Response:

Due to time constraints and the complexity of the responses required to some of these questions, a response will be provided in the Council agenda of 3 February 2025.

Deleeze Chetcuti, Director Environment, Development & Community Services

11 QUESTIONS WITHOUT NOTICE FROM COUNCILLORS

12 QUESTIONS ON NOTICE FROM COUNCILLORS

There were no Questions on Notice from Councillors.

Public Copy

OPEN SESSION ADJOURNS

PLANNING AUTHORITY IN SESSION

13 OFFICERS REPORTS TO PLANNING AUTHORITY

13.1 PROPOSED AMENDMENT TO THE KINGBOROUGH INTERIM PLANNING SCHEME 2015 TO REZONE LAND AT 26 CRESCENT DRIVE (CT146336/1, CT146336/2 AND CT146336/3) AND 21 GEMALLA ROAD (CT187452/1), MARGATE

File Number: PSA-2024-1
Author: Adriaan Stander, Senior Strategic Planner
Authoriser: Tasha Tyler-Moore, Manager Development Services

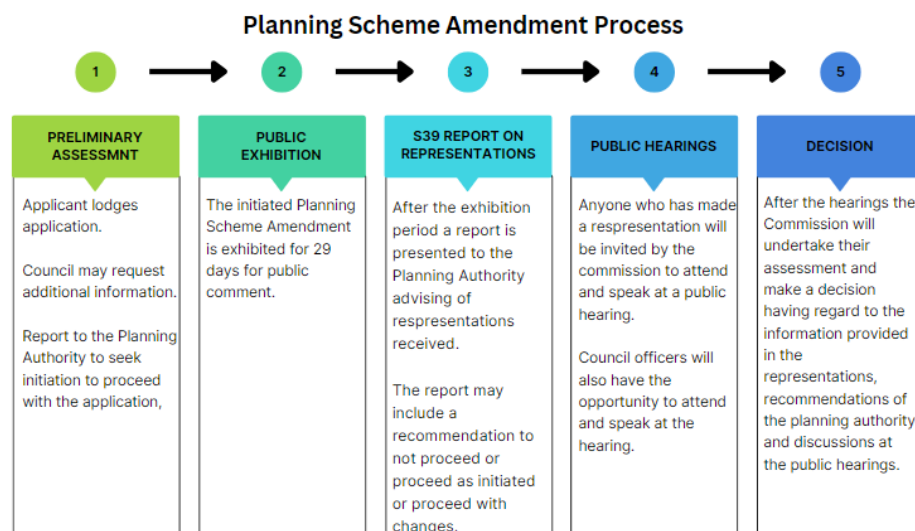
1. EXECUTIVE SUMMARY

- 1.1 The purpose of this report is to seek initiation of a planning scheme amendment as the Planning Authority to:
 - (a) rezone land at 26 Crescent Drive (CT146336/1, CT146336/2 and CT146336/3) and 21 Gemalla Road (CT187452/1), Margate from Rural Resource to General Residential;
 - (b) remove the Biodiversity Protection Overlay that applies to the subject site; and
 - (c) introduce a Specific Area Plan (SAP) to facilitate a master planned approach for the future development of the site.

- 1.5 The recommendations are that the Planning Authority resolves to initiate and certify the amendment to the Kingborough Interim Planning Scheme 2015, advise the Tasmanian Planning Commission of this decision and exhibit the initiated planning scheme amendment in accordance with the statutory process. Draft amendments are advertised for a minimum period of 28 days for public comment following initiation and certification by the Planning Authority.

- 1.6 After the exhibition period another report will be presented to the Planning Authority advising of any representations received during the exhibition period and it will include recommendations for the Tasmanian Planning Commission to consider as part of their public hearing process before a decision is made by them.

- 1.7 A brief overview of the statutory process is provided below.



2. SITE AND CONTEXT

- 2.1 The subject site comprises of 4 titles namely 26 Crescent Drive (CT146336/1, CT146336/2 and CT146336/3) and 21 Gemalla Road (CT187452/1) on the southern periphery of Margate.
- 2.2 The site measures approximately 13.18 hectares and had been used for grazing for at least 30 years. There are no buildings on the site.
- 2.3 It is zoned Rural Resource under the Kingborough Interim Planning Scheme 2015 (KIPS2015) and is proposed to be zoned Rural Zone under the Kingborough Draft Local Provision Schedule (LPS). It should be noted that the first version of the Draft LPS that was submitted to the Tasmanian Planning Commission in 2019 proposed the Future Urban Zone for the land, however the Commission did not provide support for that zoning due to fact that the land is located outside the Urban Growth Boundary which requires a different process to amend.¹
- 2.4 The site is adjoined by different zonings as follows:
- 2.5 Land to the north of the site is zoned Low Density Residential under the KIPS 2015 and proposed to be zoned General Residential under the Draft LPS.
- 2.6 Land the south of Gemalla Road is zoned Rural Living under the KIPS2015 and proposed to be zoned Rural Living under the Draft LPS.
- 2.7 Land to the east of Bundalla Road is zoned Light Industrial under the KIPS2015 and proposed to be zoned Light Industrial under the Draft LPS.
- 2.8 Land to the west opposite Channel Highway is zoned Rural Resource and Rural Living under the KIPS2015 and proposed to be zoned Rural and Rural Living under the Draft LPS.

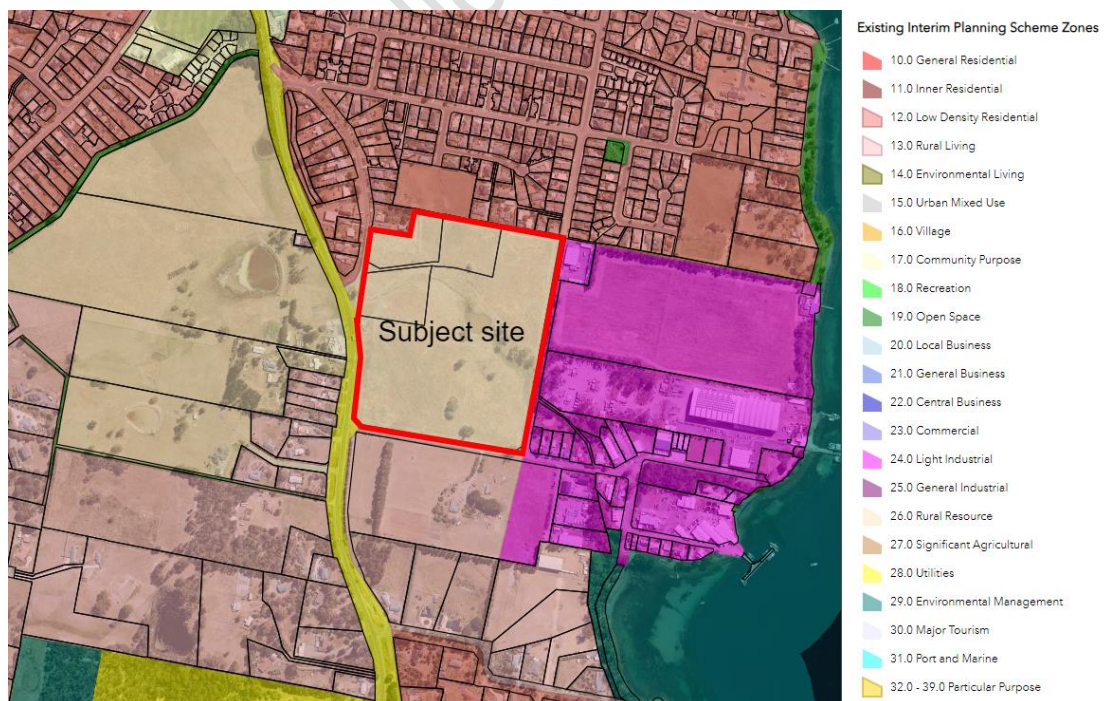


Figure 1- Existing zoning under the Kingborough Interim Planning Scheme 2015

¹ The proposal presented in this report is relying on a new provision in the Southern Tasmania Regional Land Use Strategy that allows the Planning Authority and the Tasmanian Planning Commission to consider a rezoning of land outside the Urban Growth Boundary and this is discussed in more detail under clause 5.4 and 5.5 of this report.

3. DESCRIPTION OF PROPOSAL

3.1 The proposal is for an amendment to the KIPS2015 to rezone the subject site from Rural Resource to General Residential. The proposed General Residential Zone aligns with the General Residential Zone that is proposed for Margate (directly north of the subject site) under the Kingborough Draft LPS. No subdivision or development is proposed as part of this proposal.

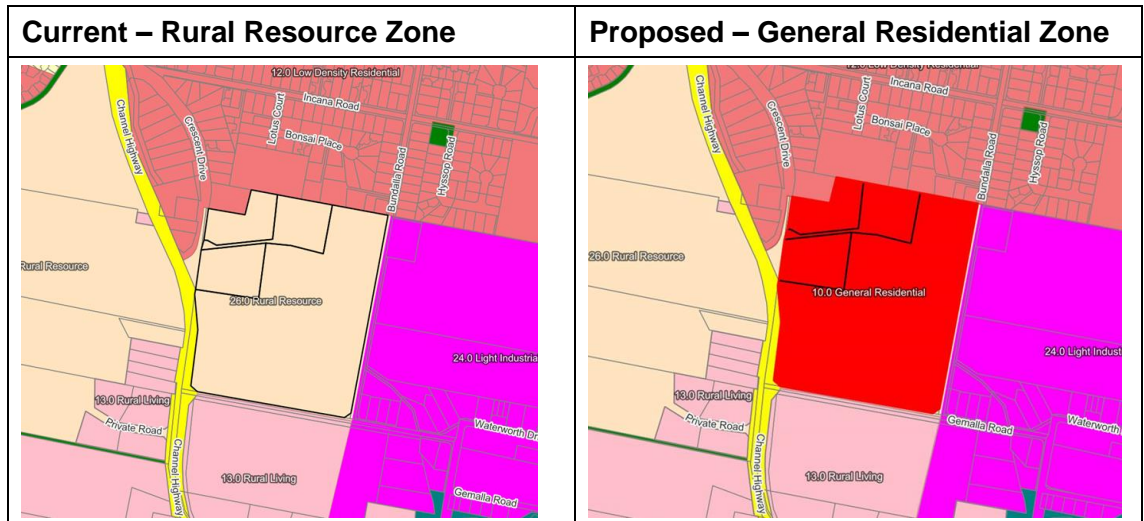


Figure 2 - Proposed zoning

3.2 The proposal is also seeking to amend the Biodiversity Code Overlay that applies to the land. Matters relating to natural values are proposed to be addressed through the provisions of a Specific Area Plan (SAP) instead of the Biodiversity Code. This approach will assist in a more flexible approach with the future subdivision design and will also facilitate a master planned approach to the future development of the site.

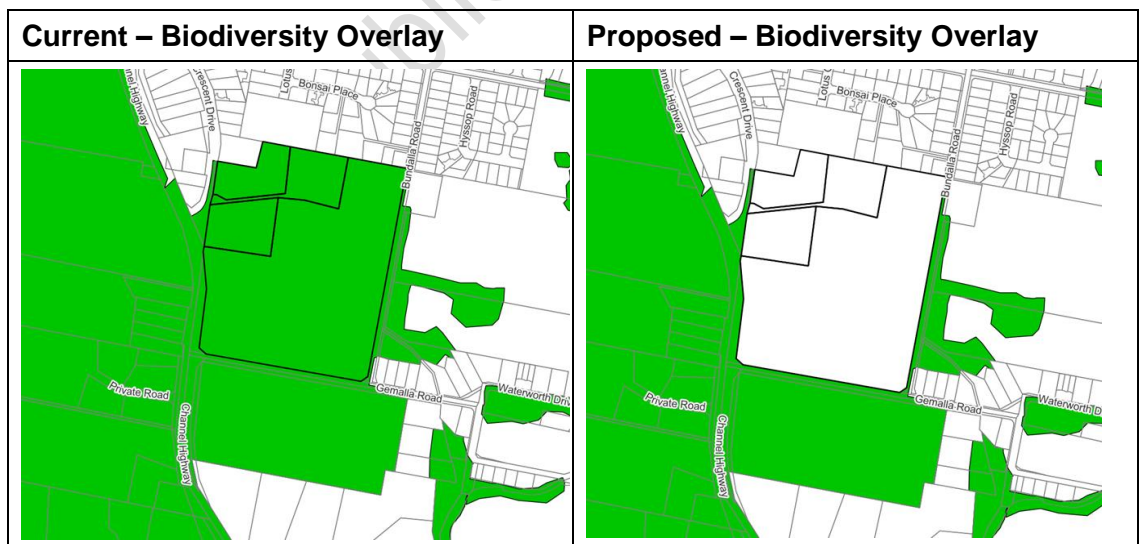


Figure 3 - Proposed amendment to Biodiversity Overlay

3.3 The proposal includes introduction of a new SAP in the Kingborough Interim Planning Scheme 2015. The proposed Gemalla Road SAP (see Attachment 1) is to ensure that future development of the land is appropriately planned, located and designed to:

- Provide for a range of housing types and densities to meet the needs of the growing population of Margate.
- Encourage new residential development with direct connectivity with the surrounding roads and open space networks.

- *Ensure that areas subject to flooding are managed to protect private property with minimal impact on natural processes.*
 - *Minimise and mitigate adverse direct and indirect impacts on natural values as a result of subdivision or development.*
 - *Manage stormwater quality and quantity to protect natural assets, infrastructure and property through the incorporation of water sensitive urban design principles.*
 - *Manage potential land use conflicts with the adjoining Light Industrial Zone.*
- 3.4 The original application proposed a reduction of the Waterway and Coastal Protection Overlay buffer in the KIPS2015 and the application did not propose a SAP.
- 3.5 The revised proposal with the SAP as presented in this report has been developed in consultation with the applicant and provides the means to support the rezoning with appropriate provisions in place (i.e. the proposed SAP and retention of the full Waterway and Coastal Protection Overlay) to assist with the future subdivision design in a way that is consistent with the broader strategic outcomes sought for the site. This approach will also provide the applicant with a level of confidence that land can be developed in future for urban purposes before investing in more resources into a detailed subdivision design.

4. ASSESSMENT OVERVIEW

- 4.1 The application has been submitted with the following supporting documents:
- Application and planning report by Gray Planning, dated 27 February 2024, response to Information Request by Gray Planning, dated 5 August 2024;
 - Traffic Assessment by Hubble Traffic, dated February 2024;
 - Site Servicing Report, by Aldanmark, dated 4 March 2024;
 - Natural Values Assessment, by ECOtas, dated 13 June 2023;
 - Margate Residential Supply and Demand Analysis, by SGS Economic and Planning, dated 6 December 2022; and
 - Land Capability Assessment, by GES Environmental Solutions, dated December 2022.
- 4.2 The application has been assessed by having regard to the following:
- *Strategic alignment, with particular reference to the consistency of the proposal with the Southern Tasmania Regional Land Use Strategy 2010- 2035, Kingborough Land Use Strategy 2019 and Council's Strategic Plan 2020-2025;*
 - *Statutory compliance with the requirements of the former provisions of LUPAA and state policies;*
 - *Infrastructure and service provision;*
 - *Environmental impacts;*
 - *Compatibility with surrounding zoning and land uses; and*
- 4.2 The above is discussed in more detail in the following sections of this report. In short, an assessment of the application has concluded that the proposal can proceed in the manner that is presented in the recommendation.

5. STRATEGIC ALIGNMENT

[Southern Tasmania Regional Land Use Strategy 2010-2035 \(STRLUS\)](#)

- 5.1 The STRLUS is a high-level strategic policy document that facilitates and manages land use change, growth and development within Southern Tasmania. The strategy represents the agreed and approved strategic directions for the southern region and provides certainty to the broader community, infrastructure providers and governments for medium and long-term investment decisions. An assessment of the application against the broader STRLUS policies is provided in Attachment 3 and it is considered that the proposal meets the outcomes sought by those directions.
- 5.2 In terms of urban growth, the approach of the STRLUS encourages the efficient use of land and infrastructure through compact settlement strategies. The strategy stipulates that urban growth is to occur through a combination of infill and controlled greenfield development in the Urban Growth Boundary (UGB). The UGB ensures that urban development is directed to areas that are best able to be supplied with appropriate infrastructure and services. It will also continue to protect other valuable peri-urban and environmentally valuable land from urban development pressures.
- 5.3 The Strategy proposes Margate as a “*Major Satellite of Greater Hobart*” and the main purpose for a satellite centre is to “*serve daily needs of surrounding community and provide a focus for day-to-day life within a community*”. Further subdivision on the town fringes is currently restricted due to the location of the UGB, however, that may change as part of the review of the settlement strategies of STRLUS and any other changes the minister may bring forward prior to the finalisation of the review.
- 5.4 The proposal as presented in this report is relying on Clause SRD 2.12 of STRLUS to rezone land outside the UGB ahead of the completion of the above-mentioned review. SRD 2.12 allows the Planning Authority and the Tasmanian Planning Commission to consider a rezoning of land outside the UGB if it:
- a) *shares a common boundary with land for urban development within the Urban Growth Boundary; and*
 - i. *only provides for a small and logical extension, in the context of the immediate area, to land zoned for urban development beyond the Urban Growth Boundary; or*
 - ii. *does not constitute a significant increase in land zoned for urban development in the context of the suburb, or the major or minor satellite as identified in Table 3, and is identified in a contemporary settlement strategy or structure plan produced or endorsed by the relevant planning authority; and*
 - b) *can be supplied with reticulated water, sewerage and stormwater services; and*
 - c) *can be accommodated by the existing transport system, does not reduce the level of service of the existing road network, and would provide for an efficient and connected extension of existing passenger and active transport services and networks; and*
 - d) *results in minimal potential for land use conflicts with adjoining uses.*
- 5.5 Having regard to the provisions of SRD 2.12, it is considered that the application meets these requirements as follows:
- a) The site shares a common boundary with land for urban purposes within the Urban Growth Boundary; and:

- The proposal does not constitute a significant increase in land zoned for urban development in the context of Margate.
 - The site is identified in the Kinborough Land Use Strategy 2019 as an area that is earmarked for future urban development and the proposed rezoning provides for a logical extension to the existing urban area in Margate (also refer to the discussion under 5.5 of this report).
 - The residential demand and supply report submitted with the application suggests that there is a need for additional residential land in the vicinity of Margate and a [preliminary demand and supply analysis](#) completed as part of the review of the STRLUS also suggests a potential shortfall in land supply in the municipality².
- b) The site can be supplied with reticulated water, sewerage and stormwater services (additional information is provided in Section 6 of this report).
- c) Preliminary traffic modelling has concluded that the anticipated future development for the site can be accommodated within the capacity of transport and road infrastructure. The proposed SAP is introducing provisions to ensure that the future development of the site has minimal impacts on the efficiency and safety of the surrounding road networks (additional information is provided in Section 6 of this report).
- d) Land use conflict can be addressed through the provisions of the SAP that is proposed for the site as well as through the application of the Attenuation Code in the KIPS2015 (additional information is provided in Section 6 of this report).

Kingborough Land Use Strategy 2019

- 5.6 The Kingborough Land Use Strategy is a local high-level non-statutory strategic document that was developed through informal consultation over a span of 10+ years. The document provides broad land use aspirations for the municipality and also helped to inform the first version of the Draft LPS (submitted to the TPC in 2019). The revised Draft LPS that is currently on public exhibition is not entirely consistent with the outcomes anticipated by the Kingborough Land Use Strategy 2019³ because partly of the time that has passed, the decision-making by the Tasmanian Planning Commission for other Councils and reconsiderations of aspects following the post lodgement meetings with the commission. However, the objectives of the strategy are still relevant, and the broader strategic intentions have been used in a manner to inform the content of the revised Draft LPS as directed by the Tasmanian Planning Commission.
- 5.7 The strategy states that the municipality's future population growth will be accommodated within residential areas that are a mixture of greenfield development sites within the urban growth boundary (new suburban type residential areas), infill development within the existing urban areas. The document states that where urban expansion is required, the most suitable areas will be at Huntingfield (which is now underway), Margate and Snug.
- 5.8 Section 5.5 of the strategy provides a detailed overview of the long-term plan for Margate and includes a detailed justification for urban expansion as proposed by this application. The strategy recommends a master planned approach for the future urban area south of Margate and that recommendation is reinforced with the SAP that is proposed for the site as part of this application.

² The review of the STRLUS is currently underway and even though a regional wide land and supply analysis will inform the review, there will be other contributory factors that will inform changes to the settlement strategies and potential amendments to the UGB.

- 5.9 The proposal as presented in this report is therefore considered consistent with the broad strategic outcomes sought by the Kingborough Land Use Strategy 2019.

Kingborough Council's Strategic Plan 2020-2025

- 5.10 The Kingborough Strategic Plan includes three key priorities, under which there are Strategic Outcomes that have relevance to the preparation of the Kingborough Draft LPS. The key priorities are to:

- encourage and support a safe, healthy and connected community;
- deliver quality infrastructure and services; and
- sustaining the natural environment whilst facilitating development for our future.

- 5.11 The proposal is consistent with the above and furthers the objectives of the Kingborough Strategic Plan 2020–2025 and a compliance statement is provided in Attachment 4.

30-year Greater Hobart Plan

- 5.12 The 30-year Greater Hobart Plan was released in 2022, and it applies to the urban metropolitan areas of the four central Hobart councils of Clarence, Glenorchy, Hobart and Kingborough.

- 5.13 The Greater Hobart Plan seeks to:

- *ensure growth complements the city's natural setting;*
- *implement a coordinated land release program that ensures sufficient land supply;*
- *promote and incentivise a more diverse and affordable housing mix;*
- *encourage urban renewal of underutilised land for residential development;*
- *support innovative design solutions to meet a diverse range of community needs;*
- *prioritise urban consolidation to create a more walkable and accessible compact city; and*
- *enable well designed medium-density developments within existing neighbourhoods and higher density dwellings in appropriate locations.*

The plan encourages infill development closer to transport corridors and within identified densification areas. Infill development is proposed to be primarily low-impact, medium density residential dwellings, while allowing for higher density dwellings in appropriate locations (for example business districts and along corridors with high frequency public transport services).

- 5.14 Even though the 30-year Greater Hobart Plan only applies to the metropolitan parts of Kingborough (i.e. Taroona, Kingston, Kingston Beach and Blackmans Bay), the plan identifies potential for urban expansion at Margate and the intention is to pre-empt consideration of UGB adjustments in this location as part of the review of the Southern Tasmania Regional Land Use Strategy.

- 5.15 It should be noted that this proposal is relying on new provisions within the Regional Land Use Strategy that allows consideration of urban expansion without the need to amend the UGB.

State Policies

- 5.16 The applicant has addressed the proposal's alignment with the *State Coastal Policy 1996*, *State Policy on Water Quality Management 1997* and *State Policy on the Protection of Agricultural Land 2009*. The proposal is not inconsistent with the outcomes sought by the relevant state policies.

6. INFRASTRUCTURE, ROADS AND SERVICE PROVISION

- 6.1 The upgrade of the Blackmans Bay sewer treatment facility has created capacity to accommodate urban growth in the locality of Margate. The service report provided by the applicant also indicates there is adequate capacity within the water supply network to meet the demands created by the proposed by the rezoning. The application will be referred to TasWater for formal comment as part of the public exhibition process.
- 6.2 In terms of stormwater management, the proposed SAP requires that the future subdivision design of the site incorporate water sensitive urban design principles consistent with Water Sensitive Urban Design Engineering Procedures for Stormwater Management in Southern Tasmania. The SAP requires vegetated swales to mitigate inundation and to manage the disposal of stormwater within the mapped Waterway and Coastal Protection Area with gross-pollutant traps at the primary detention basin inlets.
- 6.3 The traffic assessment submitted with the application indicates that the surrounding road network can accommodate the vehicular trips anticipated by the future subdivision of the site without reducing the level of traffic efficiency of the existing road network. Traffic modelling predicts highway users will continue to receive an appropriate level of service for a Category 3 State Road network.
- 6.4 The application has been referred to the Department of State Growth (DSG) who provided preliminary advice raising concerns with the proposal related to the broader impact of the proposal on the wider road network. The application will be referred to Department of State Growth for formal comment as part of the public exhibition process. DSG indicated that road widening may be required along the Channel Highway to allow for future improvement to traffic facilities between Crescent Drive (south) and Gemalla Road, including improved active travel facilities. It is recommended that the road widening be an extension of land set aside south of Gemalla Road for the Margate to Snug Pathway by Council. This requirement can be accommodated within the proposed SAP, however a formal response to this effect is required from DSG as part of the exhibition process which may then require an amendment to the proposed SAP or as a condition as part of a future subdivision permit.

7. COMPATIBILITY WITH SURROUNDING ZONING AND LAND USES

- 7.1 The proposed rezoning is considered to reflect a logical extension of the Margate to the south and will facilitate a land use outcome that is compatible with the existing urban environment in Margate. The General Residential Zone as proposed will align with the Draft LPS General Residential Zone that is proposed for the land directly north of the subject site.
- 7.2 One of the main reasons a SAP is proposed for the site is to manage potential land use conflicts, particularly in relation to the existing Light Industrial zoned land to the east of the subject site. Zoning prevents the introduction of activities that might interfere with the predominant land use in a particular area. For example, it prevents industries from moving into a residential area, a development that, in most cases, would disrupt the quality of life in the residential neighbourhood. The same applies to the rezoning of land that would introduce uses that would not be considered compatible with industrial areas. Accommodating rezoning proposals which could facilitate incompatible uses next to existing industrial areas may therefore have unintended and unwanted impacts

on the operation and long-term viability of businesses in an industrial area. Industries facing such uncertainties may grow cautious about undertaking expansion plans or investments in their existing facilities. The danger of industrial displacement (the forced relocation of industrial firms due to land-use conflicts or other environmental disruptions) can therefore have a significant impact on the success of investment and deter future investment opportunities. These areas should be carefully protected so they can continue to provide jobs for residents long into the future.

Noting the scarcity of industrial land in the region and in particular in the Kingborough municipality, the proposed SAP is introduced to ensure that the rezoning of the land to General Residential will not have a detrimental impact on the existing and future development in the Light Industrial Zone. The SAP is essentially proposing an increased setback for sensitive land uses and will work in conjunction with the provisions of the Attenuation Code to protect the industrial land, but also to protect the amenity of future residents.

- 7.3 The road separation and applicable setback requirements will also achieve an outcome that is compatible with the Rural Resource Zone and Rural Living Zone and an appropriate design outcome can be resolved through the master planning approach that is proposed by the SAP.

8. ENVIRONMENTAL IMPACTS

- 8.1 While predominantly cleared, the site contains 19 native trees, 16 of which are of very high conservation value and provide potential habitat for hollow dwelling species.
- 8.2 Tramway Creek runs through the subject site. This Class 2 waterway currently has a 30m Waterway and Coastal Protection Area either side of the watercourse, which contains the majority of the very high conservation value trees and is important for the natural ecological function of the watercourse.
- 8.3 The original proposal included the removal of the Biodiversity Protection Area and reduction in the width of the Waterway and Coastal Protection Area from 30m either side to 20m in total, with no other provisions to ensure impacts were capable of being adequately minimised and mitigated. If the amendment had proceeded as proposed, this would likely have resulted in the loss of all but 3 of the very high conservation value trees and significantly impacted the ecological function of the watercourse. These impacts of a future subdivision are able to be adequately addressed through the retention of the 30m Waterway and Coastal Protection Area and proposed SAP which includes native vegetation provisions.
- 8.4 The [Tramway Creek Flood Study](#) identifies flooding and associated risks across the Margate area for a range of storm event probabilities. The information gathered in the report is used to inform planning decisions for the and potential mitigation strategies as required. The Flood Maps of this study have defined the flood extents for the 1% AEP storm event in the location of the subject site and it indicates that the highest risk for inundation exists in and along the 30m wide Waterway and Coastal Protection Area that traverses the site. While more detailed flood modelling is required to determine how flooding will affect future development on the site and impacts downstream, the proposed rezoning can proceed as the proposed SAP provides the means to address flooding part of a future subdivision design. The SAP requires the Waterway buffer to remain unobstructed by roads and properties and to provide:
- a continuing natural channel for stormwater flows including flooding event flows;
 - a practical pedestrian walkway and linkage through the site;
 - a passive open space area;

- for the retention of natural values; and
- area for any required detention or storage systems to prevent a higher risk of flooding to properties downstream of Bundalla Road.

9. JUSTIFICATION FOR THE PROPOSED SAP

9.1 The application is for a rezoning from the Rural Resource Zone to the General Residential Zone. The application does not involve subdivision; however, the applicant intends to subdivide the land in the future.

9.2 While there is in-principle support for the land to be rezoned for urban purposes, there are a number of issues that must be addressed to demonstrate the suitability of the land for urban purposes. However, there is no opportunity under the LUPAA to condition a rezoning application and as such a SAP is required to:

- a) ensure that matters not resolved at the rezoning stage are addressed appropriately at the subdivision stage.
- b) allow Council and the Developer to work together to deliver a master plan that will optimise the yield on the land but in a manner that supports the delivery of a high-quality urban environment for existing and future residents of the area while respecting the natural values and constraints on the site.

9.3 There are no provisions in the Kingborough Interim Planning Scheme 2015 that facilitate a master-planned approach for the future development of the site, however section 30O of the former provisions of LUPAA provides an opportunity to insert a local provision in the planning scheme to achieve such an outcome.

The requirements are that an amendment, of a planning scheme, that would amend a local provision of the scheme or insert a new provision into the scheme may only be made if –

- a) *the amendment is not such that the local provision as amended or inserted would be directly or indirectly inconsistent with the common provisions, except in accordance with section 30EA, or an overriding local provision; and*
- b) *the amendment does not revoke or amend an overriding local provision; and*
- c) *the amendment is not to the effect that a conflicting local provision would, after the amendment, be contained in the scheme.*

9.4 The proposed SAP includes unique controls that will operate in conjunction with the standards provisions in the planning scheme. These controls will not:

- a) override or be inconsistent with the common provisions.
- b) revoke or amend local provisions; or
- c) result in conflicting provisions in the scheme.

9.5 Unlike the requirements for SAPs under the Local Provisions Schedules of the Tasmanian Planning Scheme, the former provisions of LUPAA does not explicitly require justification for a proposed SAP under the Interim Planning Scheme, however Section 32(f) of the former provisions of LUPAA requires that planning scheme amendments must have regard to the impact that the use and development permissible under the amendment will have on the use and development of the region as an entity in environmental, economic and social terms.

The proposed SAP is considered essential to address matters relating to natural values protection, hazard mitigation, onsite stormwater treatment and land use conflicts. It

provides a tailored framework with clear development controls to ensure improved urban outcomes that meet local needs and align with broader goals of the regional land use strategy which aims to deliver long-term environmental, economic social benefits.

- 9.6 The SAP will provide additional provisions in the General Residential Zone to facilitate the future development of the land through a master-planned approach, to ensure that the subdivision design provides an outcome that:
- a) integrates with existing development on adjoining the land;
 - b) provides a lot layout with a range of lot sizes to suit the construction of different housing typologies;
 - c) provides road connections to the adjoining road network demonstrating a clear road hierarchy;
 - d) provides public open space that is designed in a manner that is aligned with the natural flow path of the Tramway Creek and incorporates the recommendations of Council's Public Open Space Strategy;
 - e) provides pedestrian and cycle paths that connect to adjoining networks incorporating the recommendations of Council's footpath and cycle strategies;
 - f) avoids or mitigates the presence of any natural hazards on site;
 - g) reduces the risk of inundation of properties in the broader Tramway Creek catchment area;
 - h) minimises and mitigates adverse direct and indirect impacts on natural values;
 - i) provides for adequate on-site stormwater management; and
 - j) avoids or mitigates land use conflicts.

10. STATUTORY COMPLIANCE

Ability to amend the planning scheme

- 10.1 Pursuant to section 33(1) of the former provisions of LUPAA, a person may request a planning authority to amend a planning scheme administered by it.
- 10.2 This report considers the proposed amendment application as lodged by Gray Planning on behalf of Mr A Meredith and changes to the application (i.e. through the introduction of a SAP) as proposed by Council's Strategic Planning Unit in consultation with the applicant and landowner.

Assessment of planning scheme amendments

- 10.3 Pursuant to section 32(1) of the former provisions of LUPAA, a draft amendment of a planning scheme must address the following:
- Section 32(e) requires that planning scheme amendments must avoid the potential for land use conflicts in adjacent planning scheme areas. The proposed rezoning and SAP will allow urban expansion to occur, but in a manner that will avoid land use conflicts. This is discussed in more detail in clause 5.5(e) of this report.
 - Section 32(ea) requires that planning scheme amendments must not conflict with the requirements of section 30O of the former provisions of LUPAA. In turn, Section 30O requires that an amendment to an interim planning scheme must as far as practicable be consistent with the regional land use strategy. The

strategic alignment with the Southern Tasmania Regional Land Use Strategy is addressed in Attachment 3 of this report. The proposal is aligned with the strategic outcomes sought.

- Section 32(f) of the former provisions of LUPAA requires that planning scheme amendments must have regard to the impact that the use and development permissible under the amendment will have on the use and development of the region as an entity in environmental, economic and social terms. This is discussed under section 9.5 of this report.

10.4 In addition to the above, Section 33(2B)(ab) of the former provisions of LUPAA requires that any representations made under section 30I of the former provisions of LUPAA, and any statements in a report under section 30J of the former provisions of LUPAA as to the merit of a representation, that may be relevant to the amendment application, must be considered. No representations were received during the exhibition of the Scheme which are relevant to the proposed amendment, therefore sections 30I and 30J of the former provisions of LUPAA have been satisfied.

10.5 LUPAA requires that planning scheme amendments must seek to further the objectives of Schedule 1 of the former provisions of LUPAA. A detailed response to each of the objectives is provided in Attachment 5 and it has determined that the proposal meets all these objectives.

10.6 The Transitional Provisions under Schedule 6 of LUPAA will be utilised to carry the proposed SAP over to the Kingborough Draft LPS / Tasmanian Planning Scheme if it is approved by the Commission.

11. PUBLIC EXHIBITION

11.1 If Council initiates the proposed amendment, it must also certify the draft amendment in accordance with section 35 of the former provisions of LUPAA. Section 38 of the former provisions of LUPAA requires that the proposal be advertised for a minimum period of 28 days.

11.2 It is proposed to publicly exhibit the planning scheme amendment with notification:

- a) on the Kingborough Council website;
- b) twice in a newspaper circulating in the area, with one notice to be on a Saturday; and
- c) a site notice during the public exhibition period;
- d) in writing to owners of the property and adjoining properties.

11.3 The exhibition material will be made available for viewing on the Kingborough Council website and at Customer Service at the Civic Centre in Kingston and the Council Service Centre in Alonnah.

11.4 After the exhibition period Council officers will review all submissions to the planning scheme amendment and report them to the Tasmanian Planning Commission. The report will include the planning authority's views on the merit of each representation, whether the amendment should be modified and the impact of the representation on the amendment.

12. CRITICAL DATES/TIMEFRAMES

12.1 If Council supports the amendment and initiates and certifies the amendment for public exhibition, it must advise the Commission within seven days.

- 12.2 Post public exhibition, Council has 35 days from the close of the notification period to forward its report to the Commission.
- 12.3 The Commission must complete its consideration and decision process within three months of receiving Council's report on the representations, unless an extension of time has been agreed by the Minister.
- 12.4 If the Commission approves the amendment, the amendment takes effect seven days after being signed by the Commission, unless a date is specified.

13. CONCLUSION

- 13.1 The proposal as presented in this report has been developed in consultation with the applicant and will deliver an outcome that is consistent with the broader strategic outcomes sought for the site.
- 13.2 The application is considered to demonstrate compliance and consistency with the requirements and the considerations of the State's Land Use Planning System. On this basis, the proposed application is supported

14. RECOMMENDATION

That Council resolves that:

- (a) Pursuant to section 34(1)(a) of the former provisions of the *Land Use Planning and Approvals Act 1993*, Council initiates Amendment PSA-2024-1 to the Kingborough Interim Planning Scheme 2015 as per Attachment 1.
- (b) Pursuant to section 35 of the former provisions of the *Land Use Planning and Approvals Act 1993*, Council certifies that Amendment PSA-2024-1 to the Kingborough Interim Planning Scheme 2015 meets the requirements of section 32 of the former provisions of the *Land Use Planning and Approvals Act 1993* and authorises the Chief Executive Officer to sign the Instrument of Certification provided in Attachment 2.
- (c) Pursuant to section 35(4) of the former provisions of the *Land Use Planning and Approvals Act 1993*, Council will forward a copy of the draft amendment and the Instrument of Certification to the Tasmanian Planning Commissions within 7 days of certification.
- (e) Pursuant to section 38 of the former provisions of the *Land Use Planning and Approvals Act 1993*, Council will place Amendment PSA-2024-1 to the Kingborough Interim Planning Scheme 2015 on public exhibition for a period of at least 28 days following certification.

ATTACHMENTS

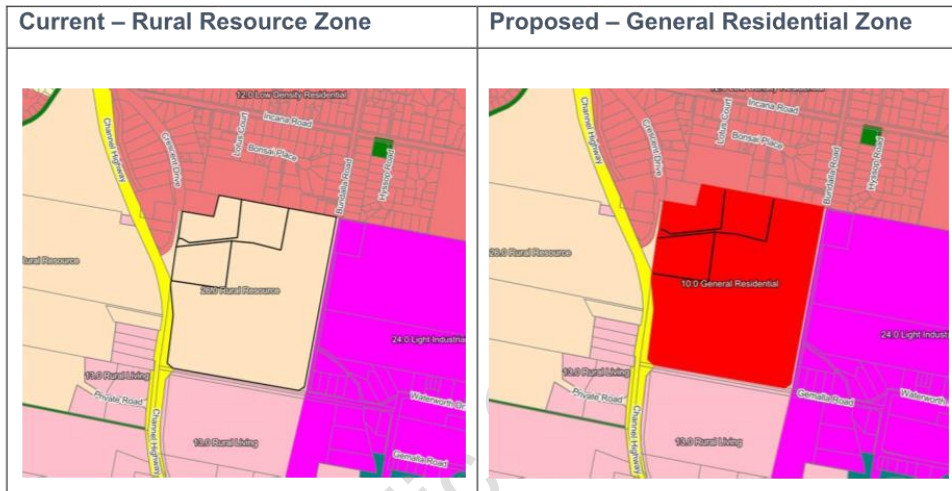
1. **Draft Amendment to the KIPS2015**
2. **Certification**
3. **STRLUS Compliance Statement**
4. **Kingborough Strategic Plan Compliance Statement**
5. **Objectives of LUPAA Compliance Statement**

Kingborough

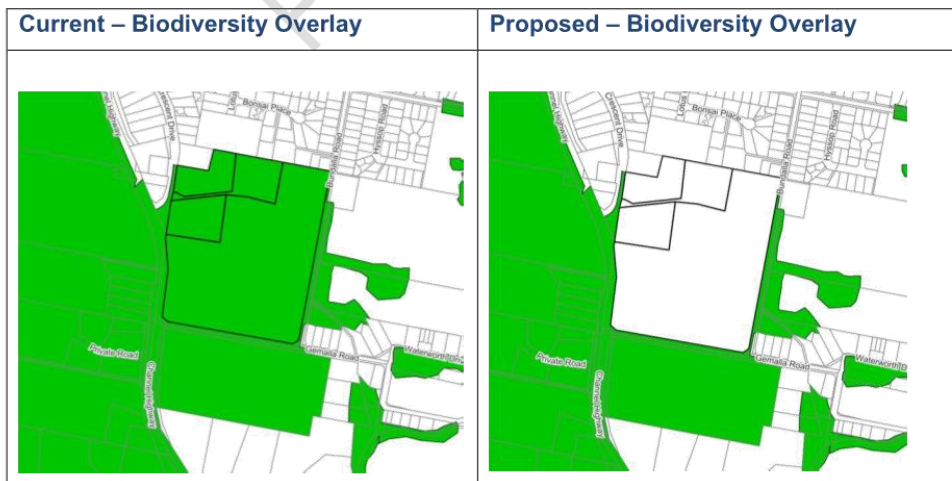
Kingborough Interim Planning Scheme 2015

Draft Amendment PSA-2024-1

1. Rezone land at 26 Crescent Drive (CT146336/1, CT146336/2 and CT146336/3) and 21 Gemalla Road (CT187452/1), Margate from Rural Resource to General Residential, as shown below:





2. Remove the Biodiversity Protection Overlay that applies to land at 26 Crescent Drive (CT146336/1, CT146336/2 and CT146336/3) and 21 Gemalla Road (CT187452/1), Margate as shown below:



Kingborough Council

 Civic Centre, 15 Channel Highway, Kingston TAS 7050
 (03) 6211 8200

 kc@kingborough.tas.gov.au

 Locked Bag 1, Kingston TAS 7050
 www.kingborough.tas.gov.au

3. Insert the Gemalla Road Specific Area Plan, as per the wording provided below:

F6.0 Gemalla Road Specific Area Plan

F6.1 Plan Purpose

The purpose of the Gemalla Road Specific Area Plan is to ensure that future development of the land is appropriately planned, located and designed to:

- F6.1.1 Provide for a range of housing types and densities to meet the needs of the growing population of Margate.
- F6.1.2 Encourage new residential development with direct connectivity with the surrounding roads and open space networks.
- F6.1.3 Ensure that areas subject to flooding are managed to protect private property with minimal impact on natural processes.
- F6.1.4 Minimise and mitigate adverse direct and indirect impacts on natural values as a result of subdivision or development.
- F6.1.5 Manage stormwater quality and quantity to protect natural assets, infrastructure and property through the incorporation of water sensitive urban design principles.
- F6.1.6 Manage potential land use conflicts with the adjoining Light Industrial Zone.

F6.2 Application of this Plan

- F6.2.1 The specific area plan applies to the area of land designated as the Gemalla Road Specific Area Plan on the overlay maps and Figure F.1.
- F6.2.2 In the area of land to which this plan applies, the provisions of the specific area plan are in substitution for, and/or in addition to the provisions of the:
 - a) General Residential Zoneas specified in the relevant provision.

F6.3 Local Area Objectives

This sub-clause is not used in this specific area plan.

F6.4 Definition of Terms

In this Specific Area Plan, unless the contrary intention appears:

Terms	Definition
Offsets	means measures implemented in accordance with Kingborough Biodiversity Offset Policy, that compensate for the residual adverse impacts of an action on the environment, when alternatives and options to avoid those impacts have been exhausted and it is still considered desirable for other economic, social, or environmental reasons for the action to proceed.
Tramway Waterway and Coastal Protection Area	means land within the 30m buffer area of the Waterway and Coastal Protection Area shown on the planning scheme maps.

F6.5 Development Standards for Subdivision

F.6.5.1 This clause is in addition to the General Residential Zone – Clause 10.6.1 Lot design

Objective:	That the design of subdivision of land is co-ordinated with adjoining land and provided with infrastructure to ensure sustainable urban and environmental outcomes and to mitigate potential land use conflicts.
Acceptable Solutions	Performance Criteria
<p>A1</p> <p>No acceptable solution.</p>	<p>P1</p> <p>Subdivision design must be in accordance with a Master Plan that:</p> <ul style="list-style-type: none"> (a) integrates with existing residential development in the area; (b) provides a lot layout with a range of lot sizes to suit different housing types; (c) provides road connections to the adjoining road network demonstrating a clear road hierarchy; (d) provides pedestrian and cycle paths that connect to adjoining networks incorporating the recommendations of Council’s footpath and cycle strategies; (e) avoids or mitigates the presence of flooding on site; (f) avoids inundation of properties in the broader Tramway Creek catchment area outside the development site; (g) provides for adequate on-site stormwater management, incorporating water urban design principles;

	<ul style="list-style-type: none"> (h) provides public open space that is designed to incorporate the Tramway Creek Waterway and Coastal Protection Area and recommendations of Council's Public Open Space Strategy; (i) minimises and mitigates adverse direct and indirect impacts on natural values and where appropriate incorporate existing native trees within the public open space; and (j) avoids or mitigates land use conflicts with the adjoining Light Industrial Zone.
<p>A2 No acceptable solution</p>	<p>P2 Subdivision must incorporate flood mitigation and stormwater measures and a stormwater disposal system that maintains, protects and enhances the natural flow and water quality values of the Tramway Creek Catchment having regard to:</p> <ul style="list-style-type: none"> (a) water sensitive urban design principles; and (b) the topography of the land and its natural pattern of drainage.
<p>A3 Each lot, or a lot proposed in a plan of subdivision, within a distance of 30m from the Bundalla Road frontage, must be for the creation of a lot:</p> <ul style="list-style-type: none"> (a) where a building for a sensitive use can be located entirely outside the 30m distance from Bundalla Road frontage; or (b) not be for the creation of a lot intended for a sensitive use. 	<p>P3 Each lot, or a lot proposed in a plan of subdivision, within a distance a distance of 30m from the from the Bundalla Road frontage, must not result in the potential for a sensitive use to be impacted by emissions, having regard to:</p> <ul style="list-style-type: none"> (a) the nature of the existing activity in the Light Industrial Zone and potential to cause emissions, including: <ul style="list-style-type: none"> (i) operational characteristics of the activity; (ii) scale and intensity of the activity; and (iii) degree of emissions from the activity; and (b) the potential future use of land in the Light Industrial Zone; (c) the intended use of the proposed lot; and

	(d) recommendations from a suitably qualified person for mitigation of emissions.
<p>A4</p> <p>Each lot, or a lot proposed on a plan of subdivision, must not impact an individual tree with a diameter >25cm or require the removal of native vegetation within the mapped vegetation area (Figure F2).</p>	<p>P4</p> <p>Each lot, or a lot proposed in a plan of subdivision, must have sufficient useable area and dimensions suitable for its intended use, having regard to minimising and mitigating adverse impacts on vegetation including by not limited to mechanisms on the title to retain trees or the application of offsets.</p>

F6.6 Tables

This sub-clause is not used in this specific area plan.

Figure F1 – Gemalla Road Specific Area Plan

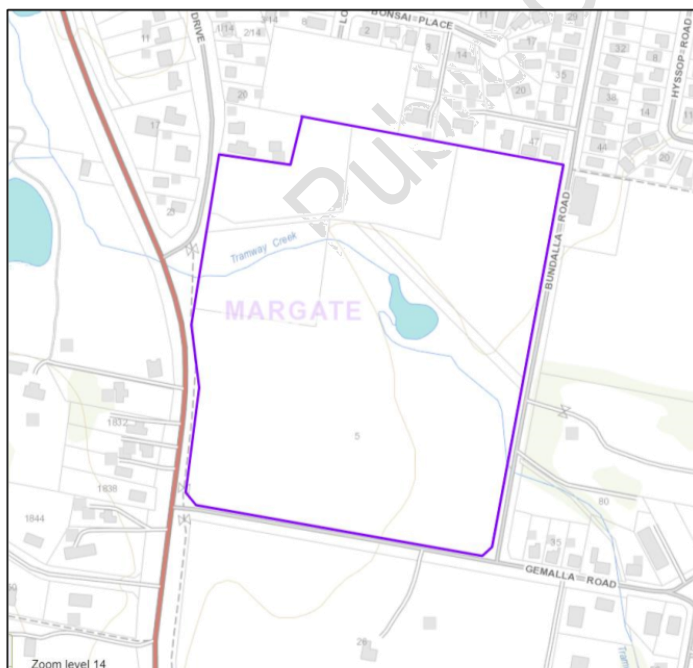


Figure F2 – Gemalla Road Specific Area Plan Mapped Vegetation Area



Kingborough

Kingborough Interim Planning Scheme 2015

INSTRUMENT OF CERTIFICATION

PSA2024-1

It is hereby certified that draft Amendment PSA2024-1 to the *Kingborough Interim Planning Scheme 2015* meets the requirements in section 32 of the former provisions of the *Land Use Planning and Approvals Act 1993*.

Public Copy

The Common Seal of the Kingborough Council is affixed hereto, pursuant to the Council's resolution on 22 January 2025, in the presence of:

..... Councillor

..... Chief Executive Officer
..... Date

Attachment 3 – Compliance with STRLUS

Table 1: STRLUS Regional Policies – Biodiversity and Geodiversity

Strategy reference	Regional Policy	Council officer assessment and response
BNV 1.1	<p>Manage and protect significant native vegetation at the earliest possible stage of the land use planning process. Where possible, ensure zones that provide for intensive use or development are not applied to areas that retain biodiversity values that are to be recognised and protected by Planning Schemes.</p>	<p>The proposal includes a Specific Area Plan (SAP) that provides provisions to minimise and mitigate adverse direct and indirect impacts on natural values as a result of future development on the site. In the absence of the SAP, this policy cannot be achieved.</p>
BNV 1.2	<p>Recognise and protect biodiversity values deemed significant at the local level and ensure that planning schemes:</p> <p>Specify the spatial area in which biodiversity values are to be recognised and protected (either by textural description or map overlay); and</p> <p>Implement an ‘avoid, minimise, mitigate’ hierarchy of actions with respect to development that may impact on recognised and protected biodiversity values.</p>	<p>The Natural Values Assessment (NVA) (ECOtas, 13 June 2023) submitted with the application concludes that no part of the subject land warrants application of the Priority Vegetation Area Overlay.</p> <p>It is agreed that the vegetation on the site does not meet (a)-(c) of the definition of Priority Vegetation. However, the eucalypt trees on the site are construed as native vegetation of local importance given their maturity and potential to provide habitat for threatened fauna. As detailed in the NVA, all but three (3) of the native trees present on the subject land meet the definition of a high conservation value tree, which is a moderate priority biodiversity value.</p> <p>In light of the above, it is agreed the application of the Biodiversity Protection Overlay across the whole site is not warranted and that the vegetation provisions as proposed in the SAP apply to those parts of the site containing high conservation value trees. Those parts of the site containing high conservation value trees have been spatially identified in Figure 2 of the SAP. The spatial identification is based on the application of a hexagonal grid capturing the maximum tree protection zone (TPZ) of individual native trees identified in the NVA using aerial imagery. While the proposed spatial area extends beyond the TPZ in some areas, this reflects the application of the hexagonal grid and also ensures any improvements in the accuracy of aerial imagery do not result in high conservation value trees being excluded from</p>

		the SAP. In the absence of the SAP, this policy cannot be achieved.
BNV 1.3	<p>Provide for the use of biodiversity offsets if, at the local level, it is considered appropriate to compensate for the loss of biodiversity values where that loss is unable to be avoided, minimised or mitigated. Biodiversity offsets:</p> <p>Are to be used only as a 'last resort'.</p> <p>Should provide for a net conservation benefit and security of the offset in perpetuity.</p> <p>Are to be based upon 'like for like' wherever possible.</p>	<p>The proposal is to include the biodiversity offset provisions in the proposed SAP to compensate for a loss of environmental values. Council has an offset policy and established procedures to ensure consistency, transparency and accountability in the application of offsets. In the absence of the SAP, this policy cannot be achieved.</p>
BNV 1.4	<p>Manage clearance of native vegetation arising from use and development in a manner that is generally consistent across the region but allowing for variances in local values.</p>	<p>The proposed SAPs will protect important local values in accordance with this policy. In the absence of the SAP, this policy cannot be achieved.</p>
BNV 1.5	<p>Ensure vegetation clearance and/or soil disturbance is undertaken in accordance with construction management plans that minimise further loss of values and encourages rehabilitation of native vegetation.</p>	<p>The Planning Scheme allow for conditions to be applied regarding construction management.</p>
BNV 2.1	<p>Avoid the clearance of threatened vegetation communities except:</p> <p>Where the long-term social and economic benefit arising from the use and development facilitated by the clearance outweigh the environmental benefit of retention; and</p> <p>Where the clearance will not significantly detract from the conservation of that native vegetation community.</p>	<p>There are no threatened vegetation communities on the subject site.</p>
BNV 2.2	<p>Minimise clearance of native vegetation communities that provide habitat for threatened species.</p>	<p>While the individual trees on the site provide potential habitat for threatened species, these trees do not form part of a native vegetation community.</p>

Table 2: STRLUS Regional Policies – Water Resources

Strategy reference	Regional Policy	Council officer assessment and response
WR 1.1	Ensure use and development is undertaken in accordance with the State Policy on Water Quality Management.	Compliance with this policy is incorporated within the Planning Scheme and is also partly addressed through the provisions in the proposed SAP. In the absence of the SAP, this policy cannot be achieved.
WR 1.2	Incorporate total water cycle management and water sensitive urban design principles in land use and infrastructure planning to minimise stormwater discharge to rivers (particularly subdivision).	The provisions of the proposed SAP require subdivision design outcome that manages stormwater quality and quantity to protect natural assets, infrastructure and property through the incorporation of water sensitive urban design principles.
WR 1.3	Include setback requirements in planning schemes to protect riparian areas relevant to their classification under the Forest Practices System.	This is provided for by the riparian setbacks within the Planning Scheme. If the riparian setbacks were reduced as originally proposed, this policy could not be achieved.
WR 1.4	Ensure development that includes vegetation clearance and/or soil disturbance is undertaken in accordance with construction management plans to minimise soil loss and associated sedimentation of waterways and wetlands.	This would be accommodated within the permit conditions of future development on the site on a case-by-case basis.
WR 2	Manage wetlands and waterways for their water quality, scenic, biodiversity, tourism and recreational values.	This is provided for through the retention of the 30m Waterway and Coastal Protection Area and via the proposed SAP and the master planned approach for future development of the site. In the absence of the 30m buffer and SAP, this policy cannot be achieved.
WR 2.2	Provide public access along waterways via tracks and trails where land tenure allows, where there is management capacity and where impacts on biodiversity, native vegetation and geology can be kept to acceptable levels.	This is provided for in the proposed SAP and the master planned approach for future development of the site. In the absence of the SAP, this policy cannot be achieved.

WR 2.3	Minimise clearance of native riparian vegetation.	This is provided for through the Waterway and Coastal Protection Code in the Planning Scheme. The proposed SAP also recognises the importance of the waterway and requires future development of the site to be designed in a manner that meets the outcome sought by this policy. In the absence of the 30m buffer and SAP, this policy cannot be achieved.
WR 2.4	Allow recreation and tourism developments adjacent to waterways where impacts on biodiversity and native vegetation can be kept to acceptable levels.	This is provided for in the proposed SAP and the master planned approach for future development of the site. In the absence of the 30m buffer and SAP, this policy cannot be achieved.

Table 3: STRLUS Regional Policies – Managing Risks & Hazards

Strategy reference	Regional Policy	Council officer assessment and response
MRH 1.1	Provide for the management and mitigation of bushfire risk at the earliest possible stage of the land use planning process (rezoning or if no rezoning required; subdivision) by the identification and protection (in perpetuity) of buffer distances or through the design and layout of lots.	This will be achieved through the Bushfire-Prone Areas Code of the Planning Scheme.
MRH 1.2	Ensure subdivision road layout designs provide for safe exit points in areas subject to bushfire hazard.	This will be achieved through the Bushfire-Prone Areas Code of the Planning Scheme.
MRH 1.3	Allow clearance of vegetation in areas adjacent to dwellings existing at the time planning schemes based on this Strategy come into effect, in order to implement bushfire management plans. Where such vegetation is subject to a biodiversity code, the extent of clearing allowable is to be the minimum necessary to provide adequate bushfire hazard protection.	This will be achieved through the Bushfire-Prone Areas Code within the Planning Scheme and application of the vegetation controls of the proposed SAP.
MRH 1.4	Include provisions in planning schemes for use and development in bushfire prone areas based upon best practice bushfire risk mitigation and management.	This will be achieved through the Bushfire-Prone Areas Code of the Planning Scheme and implemented by the building approval process.

MRH 1.5	Allow new development (at either the rezoning or development application stage) in bushfire prone areas only where any necessary vegetation clearance for bushfire risk reduction is in accordance with the policies on biodiversity and native vegetation.	This will be achieved through the Bushfire-Prone Areas Code of the Planning Scheme and application of the vegetation controls of the proposed SAP.
MRH 2.1	Provide for the mitigation of flooding risk at the earliest possible stage of the land use planning process (rezoning or if no rezoning required; subdivision) by avoiding locating sensitive uses in flood prone areas.	This will be achieved through inundation Prone Areas Code of the Planning Scheme, the retention of the 30m Waterway and Coastal Protection Area and application of the subdivision provisions of the proposed SAP. In the absence of the 30m buffer and SAP, this policy cannot be achieved.
MRH 2.2	Include provisions in planning schemes for use and development in flood prone areas based upon best practice in order to manage residual risk.	This will be achieved through the application of the Inundation Prone Areas Code of the Planning Scheme, the retention of the 30m Waterway and Coastal Protection Area and application of the subdivision provisions of the proposed SAP. In the absence of the 30m buffer and SAP, this policy cannot be achieved.

Table 4: STRLUS Regional Policies – Cultural Values

Strategy reference	Regional Policy	Council officer assessment and response
CV 1.3	Avoid the allocation of land use growth opportunities in areas where Aboriginal cultural heritage values are known to exist.	<p>Aboriginal cultural heritage is not considered in the Tasmanian Planning Scheme and is protected by separate legislation, however it should be noted that the applicant has obtained advise from Aboriginal Heritage Tasmania (AHT) that indicates that <i>there is no known Aboriginal heritage recorded within the proposed rezoning footprint, however there are a number of significant Aboriginal heritage sites recorded in the surrounding area, including stone artefact scatters, a stone quarry (stone procurement site), shell middens and an ochre source.</i></p> <p>In their response they also indicated that their records indicate that the area encompassing the proposal has not previously been assessed and there remains the potential for additional unrecorded Aboriginal heritage in the area. It is recommended that the applicant contacts AHT in the very early stages of the master planning process to avoid impacting Aboriginal heritage.</p>

Table 5: STRLUS Regional Policies – Recreation & Open Space

Strategy reference	Regional Policy	Council officer assessment and response
ROS 1.5	Ensure residential areas, open spaces and other community destinations are well connected with a network of high-quality walking and cycling routes.	The subdivision provisions of the Planning Scheme require the provision of public open space, and this is also reflected in the proposed SAP that requires that the future subdivision application must provide public open space that is designed to incorporate the 30m Tramway Creek Waterway and Coastal Protection Area and the recommendations of Council’s Public Open Space Strategy. In the absence of the 30m buffer and SAP, this policy cannot be achieved.

Table 6: STRLUS Regional Policies – Social Infrastructure

Strategy reference	Regional Policy	Council officer assessment and response
SI 2.1	Provide flexibility in planning schemes for a variety of housing types (including alternative housing models) in residential areas.	The SAP encourages a range of housing types and densities to meet the needs of the growing population of Margate. This will be achieved through the master planning approach that is required.

Table 7: STRLUS Regional Policies – Physical Infrastructure

Strategy reference	Regional Policy	Council officer assessment and response
PI 1.1	Preference growth that utilises under-capacity of existing infrastructure through the regional settlement strategy and Urban Growth Boundary for metropolitan area of Greater Hobart.	Council supports the uptake of development opportunities within the UGB to optimise the use of existing services and infrastructure, however a balanced approach is required to facilitate both infill and greenfield development to accommodate the growing population of Kingborough. The proposal can be accommodated without the need for a significant upgrade of services or infrastructure in the context of Margate.

<p>PI 2.4</p>	<p>Use information from the Regional Land Use Strategy, including demographic and dwelling forecasts and the growth management strategies, to inform infrastructure planning and service delivery.</p>	<p>Current information in STRLUS is outdated. The proposal is relying on SRD 2.12 of the STRLUS to accommodate rezoning outside the UGB.</p> <p>The residential demand and supply report submitted with the application suggests that there is a need for additional residential land in the vicinity of Margate and a preliminary demand and supply analysis completed as part of the review of the STRLUS also suggest potential shortfall in land supply in the municipality.</p> <p>The review of the STRLUS is currently underway the above-mentioned supply analysis will inform the review, there will be other contributory factors that will inform changes to the settlement strategies and potential amendments to the UGB.</p>
----------------------	--	--

Table 8: STRLUS Regional Policies – Land Use and Transport Integration

Strategy reference	Regional Policy	Council officer assessment and response
<p>LUTI 1.11</p>	<p>Encourage walking and cycling as alternative modes of transport through the provision of suitable infrastructure and developing safe, attractive and convenient walking and cycling environments.</p>	<p>The proposed SAP requires that the future subdivision design of the site provide pedestrian and cycle paths that connect to adjoining networks incorporating the recommendations of Council's footpath and cycle strategies;</p>

Table 9: STRLUS Regional Policies – Productive Resources

Strategy reference	Regional Policy	Council officer assessment and response
<p>PR 2.3</p>	<p>Utilise the settlement strategy to assess conversion of rural land to residential land through rezoning, rather than the potential viability or otherwise of the land for particular agricultural enterprises.</p>	<p>The settlement strategies within the STRLUS are currently being reviewed. Even though this is not relevant to this policy, it should be noted that the application is supported with a Land Capability Assessment that indicates that the site is predominantly classified as Class 5 and E due to being located close to Margate. None of the land examined is prime agricultural land as defined under the State Protection of Agricultural Land Policy 2009, and none of the land is classified of land as local or regionally significance. The assessed land only has limited agricultural potential and is unlikely to sustain economically viable agricultural enterprises.</p>

Table 10: STRLUS Regional Policies – Industrial Activity

Strategy reference	Regional Policy	Council officer assessment and response
IA 1	Identify, protect and manage the supply of well-sited industrial land that will meet regional need across the 5-, 15- and 30-year horizons.	The proposed SAP includes provisions to manage potential land use conflicts with the adjoining Light Industrial Zone.

Table 11: STRLUS Regional Policies – Settlement and Residential Development

Strategy reference	Regional Policy	Council officer assessment and response
SRD 1.1	Implement the Regional Settlement Strategy and associated growth management strategies through planning schemes.	The Regional Settlement Strategy of STRLUS is outdated. The proposal does not constitute a significant increase in land zoned for urban development in the locality as it presents an increase of approximately 10% urban zoned land in the context of Margate. The land is also identified in the Kinborough Land Use Strategy 2019 as an area that is earmarked for future urban development and the proposed rezoning provides for a logical extension to the existing urban area in Margate that will not have a significant impact on the outcomes sought for the regional settlement strategies.
SRD 2.1	Ensure residential growth for Greater Hobart occurs through 50% infill development and 50% greenfield development.	Land is zoned in a manner that is consistent with achieving the desired results in SRD 2.7 below – which reflects Kingborough’s contribution to a combination of infill and greenfield development.
SRD 2.2	Manage greenfield growth through an Urban Growth Boundary, which sets a 20 year supply limit with associated growth limits on dormitory suburbs.	This application is relying on SRD 2.12 which allows for the consideration of urban zoned land outside the Urban Growth Boundary.
SRD 2.8	Aim for the residential zone in planning schemes to encompass a 10-to-15-year supply of greenfield residential land when calculated on a whole of settlement basis for Greater Hobart.	The STRLUS review is currently underway, and the outcome of that review may result in changes to in Kingborough to meet this requirement. The proposal relies of SRD 2.12.
SRD 2.11	Increase the supply of affordable housing.	This is not a matter that can be addressed by the planning scheme. Other than by ensuring that there is generally sufficient suitably zoned land available for future residential development.

<p>SRD 2.12</p>	<p>Notwithstanding SRD 2.2 and SRD 2.8, and having regard to the strategic intent of the Urban Growth Boundary under SRD 2 to manage and contain growth across the Greater Hobart, land outside the Urban Growth Boundary shown in Map 10 may be considered for urban development if it:</p> <ul style="list-style-type: none"> a) shares a common boundary with land for urban development within the Urban Growth Boundary; and <ul style="list-style-type: none"> i. does not constitute a significant increase in land zoned for urban development in that locality; and ii. Is identified in a settlement strategy or structure plan produced or endorsed by the relevant planning authority; or b) Only provides for a minor and logical extension to urban development beyond the Urban Growth Boundary; and c) can be supplied with reticulated water, sewerage and stormwater services; and d) is aligned with the capacity of transport and road infrastructure and minimises impacts on the efficiency and safety of road networks; and e) results in minimal potential for land use conflicts with adjoining uses. 	<p>Having regard to the provisions of SRD 2.12, it is considered that the application meets these requirement as follows:</p> <ul style="list-style-type: none"> a) The site shares a common boundary with land for urban purposes within the Urban Growth Boundary; and: <ul style="list-style-type: none"> • The proposal does not constitute a significant increase in land zoned for urban development in the context of Margate. • The site is identified in the Kinborough Land Use Strategy 2019 as an area that is earmarked for future urban development and the proposed rezoning provides for a logical extension to the existing urban area in Margate (also refer to the discussion under 5.5 of this report). • The residential demand and supply report submitted with the application suggests that there is a need for additional residential land in the vicinity of Margate and a preliminary demand and supply analysis completed as part of the review of the STRLUS also suggests a potential shortfall in land supply in the municipality¹. b) The site can be supplied with reticulated water, sewerage and stormwater services (additional information is provided in Section 6 of this report). c) Preliminary traffic modelling has concluded that the anticipated future development for the site can be accommodated within the capacity of transport and road infrastructure. The proposed SAP is introducing provisions to ensure that the future development of the site gas minimal impacts on the efficiency and safety of the surrounding road networks (additional information is provided in Section 6 of this report). d) Land use conflict can be addressed through the provisions of the SAP that is proposed for the site as well as through the application of the Attenuation Code in the KIPS2015.
------------------------	--	---

¹ The review of the STRLUS is currently underway and even though a regional wide land and supply analysis will inform the review, there will be other contributory factors that will inform changes to the settlement strategies and potential amendments to the UGB.

Attachment 4 – Compliance with Kingborough’s Strategic Plan

Kingborough Strategic Plan	Officer assessment and comment
<p>Encourage and support a safe, healthy and connected community</p> <ul style="list-style-type: none"> • A Council that engages with and enables its community. • An inclusive community that has a strong sense of pride and local identity. • A resilient community with the capacity to flourish. • A Council that acknowledges the existence of climate change and a biodiversity emergency and has in place strategies to respond. • An active and healthy community, with vibrant, clean local areas that provide social, recreational and economic opportunities. 	<p>Strategic Outcome 1.1 aims to support increased public participation, and the Planning Scheme Amendment will be exhibited for public comment.</p> <p>Strategic Outcome 1.2 aims to generate a greater sense of local ownership within the community, and the proposed SAP achieves this by enabling and requiring a high-quality development outcome that protects local values.</p> <p>Strategic Outcome 1.3 aims to support greater capacity within local communities to prosper, and the proposal will result in additional home and the construction associated with these dwellings will contribute to the job opportunities within the municipality.</p> <p>Strategic Outcome 1.4 aims to prepare the local community for future climate change and the loss of local biodiversity, and the provisions of the planning scheme achieves this through the appropriate use of the relevant codes, plus the application of zones and other use and development provisions of the proposed SAP.</p> <p>Strategic Outcome 1.5 aims to provide welcoming and attractive public spaces for the community, and the proposed SAP will ensure that future development of the site will achieve the outcomes sought.</p>
<p>Deliver quality infrastructure and services</p> <ul style="list-style-type: none"> • Service provision meets the current and future requirements of residents and visitors. • Infrastructure development and service delivery are underpinned by strategic planning to cater for the needs of a growing population. • Community facilities are safe, accessible and meet contemporary standards. • The organisation has a corporate culture that delivers quality customer service, 	<p>Strategic Outcome 2.1 aims to ensure that Council provides the necessary services to meet local community needs. The proposal is expected to put additional demand on services, however with a larger population in Margate it is expected that it will attract more services over time and make it more self-reliant.</p> <p>Strategic Outcome 2.2 aims to support the necessary forward planning to ensure suitable public infrastructure is provided for future needs,</p>

Kingborough Strategic Plan	Officer assessment and comment
<p>encourages innovation and has high standards of accountability.</p> <ul style="list-style-type: none"> • Council is a desirable place to work, attracting committed and engaged staff through progressive human resource practices and a positive work environment. 	<p>and the proposal achieves this through the provisions of the proposed SAP.</p> <p>Strategic Outcome 2.3 is not relevant to the proposal.</p> <p>Strategic Outcome 2.4 is not relevant to the proposal.</p> <p>Strategic Outcome 2.5 is not relevant to the Proposal</p>
<p>Sustaining the natural environment whilst facilitating development for our future</p> <ul style="list-style-type: none"> • A Council that values and prioritises its natural environment, whilst encouraging investment and economic growth. • A community that has a well-developed sense of natural and cultural heritage. • Council is able to demonstrate strong environmental stewardship and leadership. • Best practice land use planning systems are in place to manage the current and future impacts of development. • Management of environmental assets is based on professional advice and strategic planning. 	<p>Strategic Outcome 3.1 aims to ensure that there is an appropriate balance between environmental and development needs, and the achieves this through the appropriate zoning of land and the application of the codes and proposed SAPs.</p> <p>Strategic Outcome 3.2 is not applicable to the proposal.</p> <p>Strategic Outcome 3.3 aims to identify and improve the condition of natural environments (waterways, biodiversity, coasts), and the proposal achieves this through the application of the proposed SAP and relevant codes in the planning scheme.</p> <p>Strategic Outcome 3.4 aims to ensure that there is an efficient approval system in place for sustainable development within Kingborough, and the this is achieved this the provisions of the existing provisions in the planning scheme and the proposed SAP.</p> <p>Strategic Outcome 3.5 aims to ensure that the environmental quality of local areas is protected through the best information available, and this is also achieved through the provisions in the planning scheme and the proposed SAP.</p>

Attachment 5 – Compliance with Objectives of LUPAA

Schedule 1, Part 1 – Objectives	Compliance statement
(a) to promote the sustainable development of natural and physical resources and the maintenance of ecological processes and genetic diversity	The objective has been satisfactorily addressed in that the amendment will facilitate the development of site in accordance with a Master Planned approach that aims to deliver sustainable development having regard to the natural values that exists on the site.
(b) to provide for the fair, orderly and sustainable use and development of air, land and water	The proposal is consistent with the recommendations of the Kingborough Land Use Strategy 2019 that provides an orderly progression over time of land use and development in the municipality.
(c) to encourage public involvement in resource management and planning	If initiated, the planning scheme amendment will be subject to a public exhibition process as per the requirements under LUPAA.
(d) to facilitate economic development in accordance with the objectives set out in paragraphs (a), (b) and (c)	The proposed intends to deliver an increased degree of certainty for the future development potential of the site aligned with paragraphs (a), (b) and (c).
(e) to promote the sharing of responsibility for resource management and planning between the different spheres of Government, the community and industry in the State	This objective is primarily provided through legislative processes and the planning scheme amendment process involves referrals to relevant agencies and statutory public consultation processes.

Schedule 1, Part 2 – Objectives	Compliance statement
(a) to require sound strategic planning and co-ordinated action by State and local government	A sound coordinated and strategic approach has been applied in preparing the planning scheme amendment and this has been underpinned by the STRLUS.
(b) to establish a system of planning instruments to be the principal way of setting objectives, policies and controls for the use, development and protection of land	This system of planning instruments is already established by legislation to meet this objective. The proposal is aligned with this objective.
(c) to ensure that the effects on the environment are considered and provide for explicit consideration of social and economic effects when decisions are made about the use and development of land	The planning scheme provides a spatial zoning framework that considers the environmental, social and economic effects. The preparation of the planning scheme amendment and has considered environmental, social and economic implications.
(d) to require land use and development planning and policy to be easily integrated with environmental, social, economic, conservation and resource management policies at State, regional and municipal levels	The proposal is consistent with the related regional and local land use strategies. The proposed SAP does not duplicate, but supports and complements the provisions in other legislation, regulations, policies and procedures as applied at State, regional or local levels.
(e) to provide for the consolidation of approvals for land use or development and related matters, and to co-ordinate planning approvals with related approvals	The planning scheme amendment has been prepared in accordance with applicable legislation and guidelines and includes the necessary provisions that, together with the standard provision in the planning scheme, will result in an outcome that is sought by this objective.

<p>(f) to promote the health and wellbeing of all Tasmanians and visitors to Tasmania by ensuring a pleasant, efficient and safe environment for working, living and recreation</p>	<p>This expected outcome of the planning scheme amendment will inherently provide for a pleasant living and recreational environment for residents.</p>
<p>(g) to conserve those buildings, areas or other places which are of scientific, aesthetic, architectural or historical interest, or otherwise of special cultural value</p>	<p>The Local Historic Heritage Code will not apply to the site. Aboriginal cultural heritage is not considered in the Tasmanian Planning Scheme and is protected by separate legislation, however it should be noted that the applicant has obtained advise from Aboriginal Heritage Tasmania (AHT) that indicates that <i>there is no known Aboriginal heritage recorded within the proposed rezoning footprint, however there are a number of significant Aboriginal heritage sites recorded in the surrounding area, including stone artefact scatters, a stone quarry (stone procurement site), shell middens and an ochre source.</i></p> <p>In their response they also indicated that their records indicate that the area encompassing the proposal has not previously been assessed and there remains the potential for additional unrecorded Aboriginal heritage in the area. It is recommended that the applicant contacts AHT in the very early stages of the master planning process to avoid impacting Aboriginal heritage.</p>
<p>(h) to protect public infrastructure and other assets and enable the orderly provision and co-ordination of public utilities and other facilities for the benefit of the community</p>	<p>The proposal will not implicate the outcome sought under this objective.</p>
<p>(i) to provide a planning framework which fully considers land capability</p>	<p>Land capability is primarily accommodated by the way that the land has been zoned. These will establish whether the land is capable of further development and will be assessed against the various development standards within the scheme (particularly within the relevant codes and per the provision of the proposed SAP).</p>

13.2 DA-2024-239 - DEVELOPMENT APPLICATION FOR SEVEN (7) WAREHOUSES, TWO (2) SHOWROOMS AND ASSOCIATED WORKS AT 'KINGSTON TOWN', 37-59 MARANOA ROAD, KINGSTON AND ADJOINING COUNCIL ROAD RESERVE

File Number: DA-2024-239
Author: Grace Paisley, Team Leader Statutory Planning
Authoriser: Tasha Tyler-Moore, Manager Development Services

Applicant:	Matt Kennedy Drafting & Design
Owner:	Zacharia Investments Pty Ltd
Subject Site:	'Kingston Town', 37-59 Maranoa Road, Kingston and adjoining Council Road Reserve (CT 126707/0)
Proposal:	Seven (7) warehouses, two (2) showrooms and associated works
Planning Scheme:	Kingborough Interim Planning Scheme 2015
Zoning:	General Business Zone
Codes:	E2.0 Potentially Contaminated Land E3.0 Landslide Code E5.0 Road and Railway Assets E6.0 Parking and Access E7.0 Stormwater Management E10.0 Biodiversity E15.0 Inundation Prone Areas E17.0 Signs
Use Class/Category:	Storage (warehouses) Bulky Goods Sales (showrooms)
Discretions:	Clause 21.4.2 Setback P1 Clause 21.4.3 Design P1 Clause 21.4.4 Passive Surveillance P1 Clause 21.4.8 Environmental Values P1 Clause E5.5.1 Existing road accesses and junction P3 Clause E10.7.1 Buildings and works P1 Clause E17.7.1 Standards for Signs P1
Public Notification:	Public advertising was undertaken between 16 November 2024 and 29 November 2024 in accordance with section 57 of the <i>Land Use Planning and Approvals Act 1993</i>
Representations:	Three (3) opposing
Recommendation:	Approval, subject to conditions

1. PROPOSAL

1.1 Description of Proposal

The application proposes the use and development of the land at 37-59 Maranoa Road, Kingston for seven (7) warehouses (storage), two (2) showrooms (bulky good sales) and associated works. The development is proposed in the eastern corner within the existing common property portion of the site.

Warehouses 1 – 7 and Showroom 1 are developed in a linear design parallel to the northern boundary; Showroom 2 is separate to the others separated by parking and access areas, located a short distance to the south. Each warehouse has a large roller door, frontage and signage facing the proposed parking areas.

Proposed uses and floor area

The end users are not known at the time of the application, however the expected use class has been applied to each of the buildings. The expected use class assists with determining the required parking rates. NOTE: If the end user does not match the approved use class, then they are required to make an application if the use is Permitted or Discretionary or if the use is No Permit Required but has a higher parking requirement.

The uses are proposed to operate between 7.00am – 7.00pm Monday – Sunday with deliveries to occur between 6.00 am – 10.00pm Monday to Saturday.

Use and reference	Floor Area
Warehouse 1	252m ² (including 25.0m ² internal office, kitchenette and single toilet on the lower level; and a 45.39m ² mezzanine area containing and additional two offices). NOTE: The plans provided and the information in the Planning report conflict with respect to the use of the building. The plans suggest that it is a combination of ‘storage’ and ‘bulky good sales’, however the application clearly defines it as ‘warehouse’. Therefore, the parking calculations have been done on ‘storage’. It is appropriate that the permit stipulates correct uses.
Warehouses 2-6	1,262m ² (including for each warehouse: 25.0m ² internal office, kitchenette and single toilet on the lower level; and a 45.39m ² additional two offices on a mezzanine level).
Warehouse 7	420m ² (including 41.20m ² internal office, kitchenette and single toilet on the lower level; and a 55.05m ² mezzanine area (storage)).
Showroom 1	616m ² (including 34.51m ² internal office, kitchenette and single toilet on the lower level; and a 58.20m ² mezzanine area (storage)).
Showroom 2	335m ² (including 31.88m ² internal office, kitchenette and single toilet on the lower level; and a 54.54m ² mezzanine area (storage)).

Access and Parking

The existing road access for the shopping centre will be utilised for this proposal. The proposal includes the introduction of a right-of-way from the Maranoa Road access point. NOTE: The matter of the right-of-way will not be dealt with in this application, as it will be resolved in the separate subdivision application and the proposal being considered is not reliant on it whilst it is still all on the same title.

The location of the building does not interfere with the existing car parking for the shopping centre.

The proposed parking rates satisfy the requirements of the Scheme, with 47 spaces proposed. NOTE: The calculations and method of calculation provided with the application were not agreed to by the Planning Authority, however the number of spaces does meet the requirement of the Scheme.

Tenancy reference <i>Matching advertised plans</i>	Use Class	Area m ²	Parking Rate <i>Pursuant to E6.0 of the Kingborough Interim Planning Scheme 2015</i>	Parking Required
Warehouse 1	Storage	182.17 floor area; and, 69.78 for ancillary office	1 space for each 100m ² of floor area and 1 for each 40m ² of ancillary office floor area.	2 for the 'storage' use PLUS 2 spaces for the ancillary offices.
Total for Warehouse 1				4 spaces
Warehouses 2-6	Storage	910 floor area; and 352 for ancillary office	1 space for each 100m ² of floor area and 1 for each 40m ² of ancillary office floor area.	9 for the 'storage' use PLUS 9 spaces for the ancillary offices.
Total for Warehouses 2-6				18 spaces
Warehouse 7	Storage (incl Mezzanine)	378.80 storage floor area; and 41.20 for ancillary office	1 space for each 100m ² of floor area and 1 for each 40m ² of ancillary office floor area.	3 for the 'storage' use PLUS 1 space for the ancillary offices.
Total for Warehouse 7				4 spaces
Showroom 1	Bulky Goods Sales	615.87	1 space for each 50m ²	12 spaces
Showroom 2	Bulky Goods Sales	334.88	1 space for each 50m ²	7 spaces
Total for Showrooms 1 and 2				19 spaces
Total required parking spaces for whole development: 45 spaces				
Total parking spaces proposed by applicant: 47 spaces (including one access for all space). Therefore, there is a surplus of two spaces.				

Building layout and design

The buildings would be located a minimum of 3m from the Maranoa Road frontage and 3m from the northeastern boundary. All other setbacks to site boundaries exceed 80m.



Figure 1 - Site layout showing proposed development

The buildings would have a maximum height of 12m above natural ground level at the highest point due to the slope of the land, however, internal to the site would have heights around 5.7m.

The buildings would be constructed with precast concrete panelling; the frontage to the parking areas will present with a roller door and reasonable levels of glazing on the lower and upper floors (figure 2). External colours include shale grey, dark grey and natural finish concrete. The two showroom buildings have greater expanses of glazing. The rear of the building is mostly plain with brickwork/blockwork on the lower level and precast concrete for the upper level; there is limited glazing and no articulation. The two parts of the proposal that will be easily seen from outside the site (Maranoa Road) are the east elevations of Showrooms 1 and 2. The presentation to the road will be brickwork/blockwork on the lower level (to 6.1m at its highest point for showroom 1; 3.5m at its highest point for showroom 2) and a mix of glazing a precast panel for the upper level (figure 3).



Figure 2 - Proposed Southern elevation of linear building



Figure 3 - Eastern elevation, facing Maranoa Road



Figure 4 - Proposed Streetscape to Maranoa Road

Signage

External signage proposed described in the table below. NOTE: As the end users are unknown, the content of the signage is not known at the time of assessment, however these are the sign types and sizes that are proposed.

Sign Ref No.	Sign Type (Cl. E17.3)	Sign Dimension	Status of Sign in Zone (Table E17.3)
1-7	7 wall signs affixed on the southern elevation of the buildings above the roller door entries of the warehouse tenancies	1.2m high x 3.5m wide	Permitted
8	Above awning sign on the southeastern elevation of showroom 1.	2.4m high x 4.2m wide	Permitted
9	Wall sign on the southern elevation of showroom 1.	2.4m high x 4.2m wide	Permitted
10-11	2 above awning signs on the southern and northern elevations of showroom 2.	2.4m high x 4.2m wide	Permitted
12-13	2 wall signs on the eastern and western elevations of showroom 2.	2.4m high x 3m wide	Permitted

Vegetation Removal

Vegetation removal (non-native and native) is required to facilitate the development including the removal of radiata pines within the development footprint and the removal of seven (7) native trees. The native vegetation removal involves four (4) trees of very high conservation on the subject land (Trees 5, 6, 7 and 13) and the removal of a further three native trees (Trees 3, 4 and 12) trees that are not of high conservation value.

Works in adjoining road reserve

The adjoining Maranoa Road road-reserve forms part of the application as the application proposes stormwater connection works within the road reserve. Landowner consent from Council in accordance with Section 52 of the *Land Use Planning and Approvals Act 1993* was provided with the application.

1.2 Description of Site

The site is located at 37-59 Maranoa Road, Kingston which is currently occupied by the 'Kingston Town Shopping Centre'. The development is proposed within the eastern corner of the site which is void of any buildings and forms part of the site's common property. The remainder of site contains the Kingston Town Shopping Centre and associated car parking.

The site is 5.358 ha in area (including the strata titles that contain the existing shopping centre) and has frontages and existing accesses onto Channel Highway and Maranoa Road. The area of the site where the development is proposed is not developed and is largely vegetated (figure 5). This area of the site falls considerably towards the lot boundaries with a drop of approximately 7m (figure 7).

The area surrounding the site is predominately used for residential purposes in the form of either single or multiple dwellings. This is reflective of the zoning in the area as shown in figure 6. To the northeast the site is adjoined by Calvin Christian School which is located in the Community Purpose Zone.



Figure 5 - Aerial image of the subject site and surrounds with the area of the site to be occupied by the proposal circled in red (Source: Council GIS mapping).



Figure 6 - Zoning map of the subject site and surrounds (Source: Council GIS mapping).



Figure 7 - Image taken from the access to the site from Maranoa Road looking toward the development area



Figure 8: Image taken looking towards the development area from within the site taken from Eastern portion of the site.

1.3 Background

The site has a long history of development approvals in relation to the development of the Kingston Town Shopping Centre; none of which are directly related to the part of the site that the proposal will affect.

The application (DA2024-239) was supported by a number of reports including:

- Traffic Impact Assessment (TIA)
- Natural Values Assessment (NVA)
- Civil Engineering Design
- Water and Sewerage Demand Assessment
- Stormwater Assessment
- Acoustic Assessment
- Environmental Site Assessment.

NOTE: Some of the supporting documentation refers to subdivision of the land, however, the subdivision does not form part of this application and is being assessed under a separate application.

2. ASSESSMENT

2.1 State Policies and Act Objectives

The proposal is consistent with the outcomes of the State Policies, including those of the Coastal Policy.

The proposal is consistent with the objectives of Schedule 1 of the *Land Use Planning and Approvals Act 1993*.

2.2 Strategic Planning

The relevant strategies associated with the Scheme are as follows:

Zone Purpose Statements of the General Business Zone

The relevant zone purpose statements of the General Business Zone are to:

- 21.1.1.1 To provide for business, community, food, professional and retail facilities serving a town or group of suburbs.
- 21.1.1.2 To ensure the major centres provide for a range of convenience and goods and services as well as some community services and facilities for the municipal area and surrounds.
- 21.1.1.3 To provide a focus for employment at the municipal level primarily in retailing, but complemented by a range of office based employment mainly in professional and personal services.
- 21.1.1.4 To facilitate residential use above ground floor level.
- 21.1.1.5 To ensure development is highly accessible by public transport, walking and cycling.
- 21.1.1.6 To ensure the rural service centres provide for the daily and weekly needs of the community.
- 21.1.1.7 To provide for a mix of retail and office based employment servicing the local area including a supermarket and a range of specialty shops.
- 21.1.1.8 To provide a safe, comfortable and pleasant environment for workers, residents and visitors through the provision of high quality urban spaces and urban design.

Clause 21.1.2 – Local Area Objectives and Desired Future Character Statements

The Scheme details separate Local Area Objectives and Desired Future Character Statements for the main towns in the municipal area. The following Local Area Objectives and Desired Future Character Statements are relevant to the assessment of this application.

Local Area Objectives		Implementation Strategy	
KINGSTON			
(a)	A number of general business areas that are outside of the main central Kingston area will provide a range of retail and other commercial functions.	(a)	These areas should be developed so that they provide for enjoyable shopping experiences with a focus on convenience and easy access for private vehicles.
Desired Future Character Statements		Implementation Strategy	
KINGSTON			
(a)	Future development is to be consistent with a convenient shopping experience and should not adversely impact on surrounding or neighbouring uses.	(a)	Development should accommodate a high quality of landscaping and design, be set back from external boundaries and be easily accessible.

The proposal complies with the above-mentioned statements and objectives as the use and development will:

- (a) Provide for additional buildings to be used for a range of retail and other commercial functions outside of the main central Kingston area. The proposal will provide for general business and commercial functions in a location that is conveniently co-located with the Kingston Town Shopping Centre.
- (b) The tenancies have been designed to avoid and mitigate impacts to residential uses within the nearby General Residential and Inner Residential zones. The development is located away from the land in the Inner Residential Zone and setback more than the Scheme requires from the General Residential Zone.
- (c) The development would include landscaping along the Maranoa Road frontage and a setback of 3m from the frontage to respect the residential nature of the street. The building has also been setback 3m from the adjoining Community Purpose Zone, although the Scheme does not require this setback.
- (d) The development would not adversely impact on surrounding uses as it is proposed to be consistent with the purpose of the General Business Zone and relevant standards of the Scheme.

2.3 Statutory Planning

The use is categorised as Storage (warehouses) and Bulky Goods Sales (showrooms) under the Scheme, which are uses that require discretionary assessment in the General Business Zone.

Council's assessment of this proposal should also consider the issues raised in the representations, the outcomes of any relevant State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act 1993*.

2.4 Use and Development Standards

The proposal satisfies the relevant Acceptable Solutions of the Scheme (see checklist in Attachment 1), with the exception of the following:

**General Business Zone
Clause 21.4.2 Setback**

Acceptable Solution A1
Building setback from frontage must be parallel to the frontage and must be no more than: 5 m, if fronting Channel Highway. 3 m, if fronting any other street.
Performance Criteria P1
Building setback from frontage must satisfy all of the following: (a) be consistent with any Desired Future Character Statements provided for the area; (b) be compatible with the setback of adjoining buildings, generally maintaining a continuous building line if evident in the streetscape; (c) enhance the characteristics of the site, adjoining lots and the streetscape; (d) provide for small variations in building alignment only where appropriate to break up long building facades, provided that no potential concealment or entrapment opportunity is created; (e) provide for large variations in building alignment only where appropriate to provide for a forecourt for space for public use, such as outdoor dining or

landscaping, provided that no potential concealment or entrapment opportunity is created and the forecourt is afforded very good passive surveillance.
Proposal
The buildings are proposed to be setback between 3m and 5m from the Maranoa Road frontage. This is more than the 3m permitted under the Acceptable Solution. The Acceptable Solution also requires a setback of no more than 5m from Channel Highway which the proposal exceeds.

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- As mentioned earlier in the report, the proposal is considered to be consistent with the Desired Future Character Statements provided for the area.
- Although zoned General Business, the site is located within a street that includes residential uses due to the zoning of the land on the opposite side of the Maranoa Road. Whilst a small setback is generally encouraged in the General Business Zone, to ensure the development would integrate into the streetscape, the development has been setback between 3m and 5m to be reflective of the dwelling setbacks in the street.

This larger setback enables the development to include the provision of landscaping within the frontage and minimise the height of the building due to the slope of the land. The landscaping would soften the appearance of the large buildings that are commercial in design and assist in the transition to the more generous setbacks of the buildings at Calvin Christian School to the north. A condition should be included in any approval to ensure appropriate landscaping. In some circumstances substantial landscaping would not be expected, because there should be active frontages addressing the street. Because of the fall of the land, that has been difficult to achieve, resulting in long blank walls; therefore landscaping would enhance that presentation. This would not be an acceptable approach if this site was part of a greater shopping strip where the adjoining road was an active commercial street.

- Regarding the frontage setback to Channel Highway, the site is an internal lot from the Channel Highway frontage and a setback of 5m is not possible. The development is instead designed to address the Maranoa Road frontage of the site.

General Business Zone
Clause 21.4.3 Design

Acceptable Solution A1
Building design must comply with all of the following:
(a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;
(b) for new building or alterations to an existing facade provide windows and door openings at ground floor level in the front façade no less than 40% of the surface area of the ground floor level façade;
(c) for new building or alterations to an existing facade ensure any single expanse of blank wall in the ground level front façade and facades facing other public spaces is not greater than 30% of the length of the facade;
(d) screen mechanical plant and miscellaneous equipment such as heat pumps, air conditioning units, switchboards, hot water units or similar from view from the street and other public spaces;
(e) incorporate roof-top service infrastructure, including service plants and lift

<p>structures, within the design of the roof;</p> <p>(f) provide awnings over the public footpath if existing on the site or on adjoining lots;</p> <p>(g) not include security shutters over windows or doors with a frontage to a street or public place.</p>
<p>Performance Criteria P1</p>
<p>Building design must enhance the streetscape by satisfying all of the following:</p> <p>(a) provide the main access to the building in a way that addresses the street or other public space boundary;</p> <p>(b) provide windows in the front façade in a way that enhances the streetscape and provides for passive surveillance of public spaces;</p> <p>(c) treat large expanses of blank wall in the front façade and facing other public space boundaries with architectural detail or public art so as to contribute positively to the streetscape and public space;</p> <p>(d) ensure the visual impact of mechanical plant and miscellaneous equipment, such as heat pumps, air conditioning units, switchboards, hot water units or similar, is insignificant when viewed from the street;</p> <p>(e) ensure roof-top service infrastructure, including service plants and lift structures, is screened so as to have insignificant visual impact;</p> <p>(f) not provide awnings over the public footpath only if there is no benefit to the streetscape or pedestrian amenity or if not possible due to physical constraints;</p> <p>(g) only provide shutters where essential for the security of the premises and other alternatives for ensuring security are not feasible;</p> <p>(h) be consistent with any Desired Future Character Statements provided for the area.</p>
<p>Proposal</p>
<p>Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the points (b) and (c) of the Acceptable Solution as a large retaining structure is proposed at ground level.</p>

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The building has been designed to enhance the streetscape by providing the main access to the buildings in a way that addresses the street but is also addressing the internal car parking area of the proposed development and the Kingston Town Shopping Centre. The access to the buildings would be clearly visible due to the large expanses of glazing and the proposed signage making the access easily identifiable (figures 9, 10 and 11).
- As shown in figures 9, 10 and 11, the front facades of the buildings have been designed to enhance the streetscape by providing generous areas of glazing. This would ensure there is passive surveillance of Maranoa Road and the internal car parking area.
- Due to the slope of the land, the design includes large retaining structures on the front façade facing Maranoa Road. The design was amended through the application process after concerns were raised with the applicant. The amended plans now include planter boxes and landscaping in front of the retaining wall to treat the large expanses of blank wall so that it would contribute positively to the streetscape. To further improve the presentation of the large expanse of wall, a condition should be included to provide improved detail of finishes to the eastern façade of the building.

- The applicant has shown mechanical plant and miscellaneous equipment on the plans. The plans demonstrate that the visual impact from these would be minimal.
- All rooftop infrastructure would be screened from view.
- No awnings are required to be provided, and no security shutters are proposed.
- The proposed is consistent with the Desired Future Character Statements provided for the area as mentioned earlier in this report.

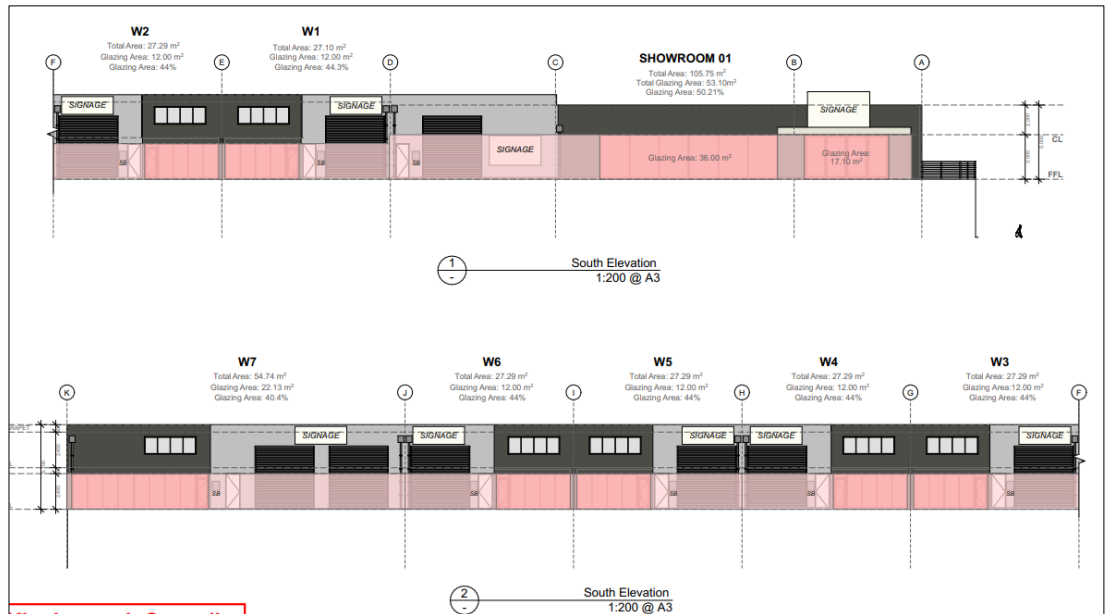


Figure 9: Elevation plan showing the areas of proposed glazing

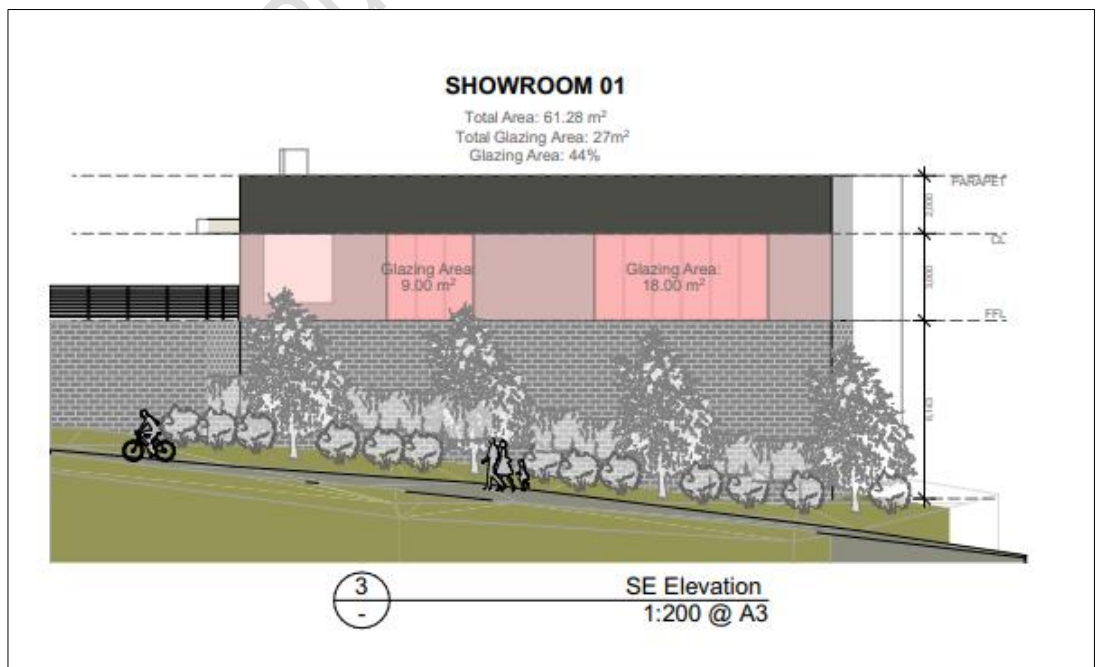


Figure 10 - Elevation plan showing the areas of proposed glazing facing Maranoa Road

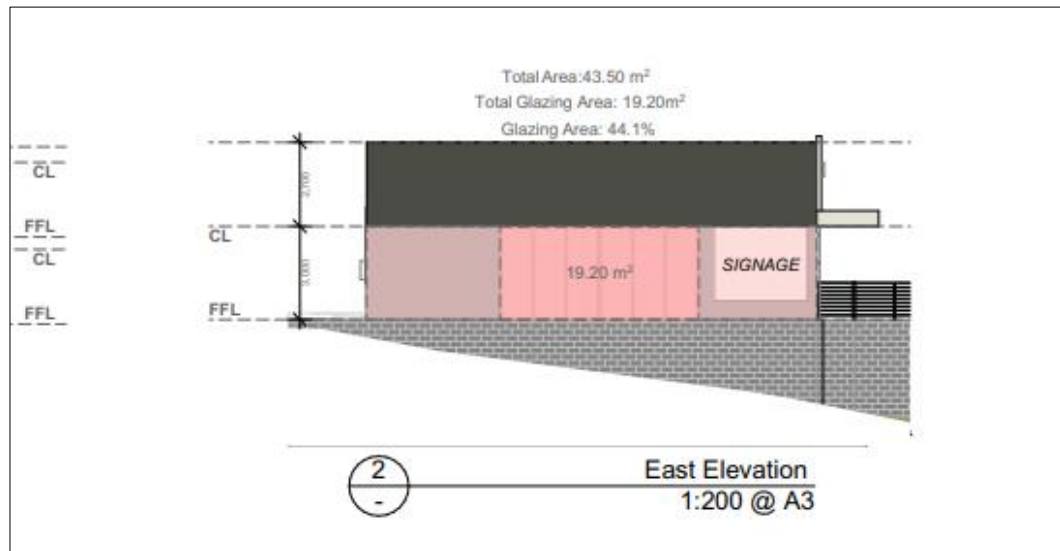


Figure 11 - Elevation plan showing the areas of proposed glazing facing Maranoa Road

General Business Zone
Clause 21.4.4 Passive Surveillance

<p>Acceptable Solution A1</p> <p>Building design must comply with all of the following:</p> <ul style="list-style-type: none"> (a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site; (b) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the front façade which amount to no less than 40% of the surface area of the ground floor level facade; (c) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the façade of any wall which faces a public space or a car park which amount to no less than 30% of the surface area of the ground floor level facade; (d) avoid creating entrapment spaces around the building site, such as concealed alcoves near public spaces; (e) provide external lighting to illuminate car parking areas and pathways; (f) provide well-lit public access at the ground floor level from any external car park.
<p>Performance Criteria P1</p> <p>Building design must provide for passive surveillance of public spaces by satisfying all of the following:</p> <ul style="list-style-type: none"> (a) provide the main entrance or entrances to a building so that they are clearly visible from nearby buildings and public spaces; (b) locate windows to adequately overlook the street and adjoining public spaces; (c) incorporate shop front windows and doors for ground floor shops and offices, so that pedestrians can see into the building and vice versa; (d) locate external lighting to illuminate any entrapment spaces around the building site; (e) provide external lighting to illuminate car parking areas and pathways; (f) design and locate public access to provide high visibility for users and provide clear sight lines between the entrance and adjacent properties and public spaces; (g) provide for sight lines to other buildings and public spaces.

<p>Proposal</p> <p>Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the points (b) and (c) of the Acceptable Solution as a large retaining structure is proposed at ground level.</p>

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- As shown in figures 9, 10 and 11, the buildings have been designed to provide for passive surveillance of public spaces with large glazing areas proposed within the facades. These windows would overlook the Maranoa Road frontage and internal public spaces. The glazing is proposed to be clear which would mean that the windows would also allow pedestrians to see into the building as well. It is acknowledged that the said glazing is not at the pedestrian level, which is not ideal, but the unusual fall of the land has made it difficult to achieve that.
- The main pedestrian entrances to the building would be clearly visible from the other buildings within the proposed development, but also from the car parking area for the existing shopping centre.
- No entrapment spaces are proposed.
- The car parking area and pathways would be illuminated in accordance with the requirements of the Parking and Access Code. It is also noted that the existing access point onto Maranoa Road has existing lighting. There is an existing street light on the opposite side of Maranoa Road where the development is proposed that illuminates the pathway.
- The layout of the development means that there would be clear sight lines from each building to the other buildings within site and the car parking area. The location of the building would also improve the passive surveillance at the rear of the existing shopping centre which has very limited passive surveillance.

General Business Zone
Clause 21.4.8 Environmental Values

<p>Acceptable Solution A1</p> <p>No trees of high conservation value will be impacted.</p>
<p>Performance Criteria P1</p> <p>Buildings and works are designed and located to avoid, minimise, mitigate and offset impacts on trees of high conservation value.</p>
<p>Proposal</p> <p>A Natural Values Assessment (NVA) (Enviro-dynamics, V4, December 2024) submitted in response to a further information request confirms that there are four (4) trees of very high conservation on the subject land, all of which are proposed for removal (Trees 5, 6, 7 and 13) for the proposed show rooms and associated infrastructure. The proposal also includes the removal of a further three native trees (Trees 3, 4 and 12). However, these trees are not of high conservation value. An arborist assessment (Tree Glider, 21 September 2024) submitted in response to a further information request confirms that trees located on adjacent land but in proximity to the proposed buildings and works are proposed for and capable of retention.</p>

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- Trees 5, 6 and 13 are directly within the footprint of the proposed buildings and services and there is no feasible option to design around these trees to enable their retention.
- An arborist assessment (Tree Glider, 21 September 2024) submitted in response to a further information request confirms that Tree 7 has been subject to significant historical encroachment which impacts on the long-term health and viability of this tree. The proposed footpath extension and widening will result in further encroachment into the structural root zone of this tree and the installation of TasNetworks and underground electricity infrastructure will cause major encroachment. Consequently, Tree 7 is not feasible for retention.
- The loss of these trees can be offset via inclusion of a condition in any planning permit issued.
- Conditions are recommended for inclusion in any permit issued approving removal of Trees 3-7 and 12-13, requiring retention of remaining trees and implementation of tree protection measures during construction and requiring payment of a financial contribution of \$570/tree of high conservation value (totalling \$2,280).

E3.0 Potentially Contaminated Land Code
Clause E2.6.2 Excavation

Acceptable Solution A1
No acceptable solution.
Performance Criteria P1
Excavation does not adversely impact on health and the environment, having regard to: (a) an environmental site assessment that demonstrates there is no evidence the land is contaminated; or (b) a plan to manage contamination and associated risk to human health and the environment that includes: i. an environmental site assessment; ii. any specific remediation and protection measures required to be implemented before excavation commences; and iii. a statement that the excavation does not adversely impact on human health or the environment.
Proposal
A service station is currently located on the site which is a potentially contaminating activity. Although the service station is located over 100m from the development site, the Code is still applicable, and the proposal involves excavation of the land therefore the application must be assessed against the Performance Criteria.

The proposed variation can be supported pursuant to this Performance Criteria of the Code for the following reasons:

- The application was supported by an Environmental Site Assessment prepared by Geo-Environmental Solutions. The assessment was reviewed by Council’s Environmental Health Officer who is satisfied with the assessment.
- The assessment states:

- Preferential groundwater flow is to the west away from the subject site.
- It is estimated that it would take over 100 years for groundwater to reach the site from the service station site. In the unlikely event that the service station has leaked undetected hydrocarbons since construction in 1999, the plume would not have reached the site by now.
- Hydrocarbons would naturally attenuate well before any impact was to reach the development site; and
- Groundwater is confined within the sandstone rock and has no pathway to contaminate surface soils.
- The assessment concludes the proposal is compliance with the potentially contaminated land code (E2.0) of the Scheme.

E5.0 Road and Railway Assets Code

Clause E5.5.1 – Existing road accesses and junction

Acceptable Solution A3
The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.
Performance Criteria P3
Any increase in vehicle traffic at an existing access or junction in an area subject to a speed limit of 60km/h or less, must be safe and not unreasonably impact on the efficiency of the road, having regard to: (a) the increase in traffic caused by the use; (b) the nature of the traffic generated by the use; (c) the nature and efficiency of the access or the junction; (d) the nature and category of the road; (e) the speed limit and traffic flow of the road; (f) any alternative access to a road; (g) the need for the use; (h) any traffic impact assessment; and (i) any written advice received from the road authority.
Proposal
The peak traffic generation would be 34 vehicles per hour which would equate to 17 inward vehicles per hour and 17 outward vehicles per hour across two accesses. This exceeds the 40 vehicle movements per day allowed under the Acceptable Solution.

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The application was supported by a TIA prepared by Midson Traffic Pty Ltd. The TIA was reviewed by Council’s Development Engineer who is satisfied with the recommendations of the report.
- The TIA included a capacity analysis for the intersections on Maranoa Road and Channel Highway to determine whether the increase in traffic generated by the development would unreasonably impact on the safety and efficiency of the roads. The traffic generation was modelled and demonstrated that it will not have a significant adverse impact on the traffic efficiency and general operation of the

road network. The Maranoa Road intersection operates at Level of Service (LOS) A or B for all approaches. This LOS is maintained as a result of the development. The intersection of Channel Highway is already at a poor level of service for right turning exit movements independent of the proposed development and the TIA states that the proposal only increases the loading of the access by 14 vehicles per hour. These 14 vehicle movements would not all be turning right and due to the location of the development on the site, it is likely that a high number of traffic movements would fall into the Maranoa Road access which has a high spare capacity. The TIA concludes that the development would not unreasonably impact on the efficiency of the road.

E10.0 Biodiversity Code
Clause E10.7.1 Buildings and works

<p>Acceptable Solution A1</p> <p>Clearance and conversion or disturbance must be within a Building Area on a plan of subdivision approved under this planning scheme.</p>
<p>Performance Criteria P1</p> <p>Clearance and conversion or disturbance must satisfy the following:</p> <p>(a) if low priority biodiversity values:</p> <ul style="list-style-type: none"> i. development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development; and ii. impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fire-resistant design of habitable buildings; and/or <p>(b) if moderate priority biodiversity values:</p> <ul style="list-style-type: none"> i. development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development; and ii. impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fire-resistant design of habitable buildings; and iii. remaining moderate priority biodiversity values on the site are retained and improved through implementation of current best practice mitigation strategies and ongoing management measures designed to protect the integrity of these values; and iv. residual adverse impacts on moderate priority biodiversity values not able to be avoided or satisfactorily mitigated are offset in accordance with the Guidelines for the use of Biodiversity Offsets in the local planning approval process, Southern Tasmanian Councils Authority, April 2013 and Kingborough Biodiversity Offset Policy 6.10, November 2023; and/or <p>(c) if high priority biodiversity values:</p> <ul style="list-style-type: none"> i. development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development; and ii. impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fireresistant design of habitable buildings; and iii. remaining high priority biodiversity values on the site are retained and improved through implementation of current best practice mitigation strategies and ongoing management measures designed to protect the integrity of these values; and

- iv. special circumstances exist; and
- v. residual adverse impacts on high priority biodiversity values not able to be avoided or satisfactorily mitigated are offset in accordance with the Guidelines for the use of Biodiversity Offsets in the local planning approval process, Southern Tasmanian Councils Authority, April 2013 and Kingborough Biodiversity Offset Policy 6.10, November 2023; and
- vi. clearance and conversion or disturbance will not substantially detract from the conservation status of the biodiversity value(s) in the vicinity of the development.

Proposal

The NVA (Enviro-dynamics, V4, December 2024) confirms there are moderate priority values within the footprint of the proposed development, including, potential habitat for the Chaostola skipper within the footprint of the building and one (1) tree of very high conservation value within the sewer alignment. The remaining trees of very high conservation value are located outside the Biodiversity Protection Area and only require assessment against Clause 21.4.8.

The proposal is unable to meet A1 as there is no building area on the title and the proposal involves clearance and conversion of moderate priority biodiversity values, being Chaostola skipper habitat and a tree of very high conservation value. Therefore, it must be assessed against the Performance Criteria.

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The proposed development is designed to minimise impacts on priority vegetation to the extent feasible, with the sewer alignment and building location limited by the topography of the subject site.
- The development is not within a Bushfire-Prone Area and there is no clearing for bushfire purposes.
- Remaining values are proposed to be maintained.
- A financial contribution for the loss of moderate priority biodiversity values will satisfactorily offset any impacts in accordance with the Guidelines for the Use of Biodiversity Offsets in the Local Planning Approval Process, Southern Tasmanian Councils Authority 2013 and Council Policy 6.10 (November 2023).
- Conditions are recommended for inclusion in any permit issued approving removal of 50m² of Chaostola skipper habitat and one (1) tree of very high conservation value, requiring retention of remaining trees and implementation of tree protection measures during construction and requiring payment of a financial contribution of \$570 for the loss of one (1) tree of very high conservation value and \$204.75 for the loss 50m² of potential Chaostola skipper habitat, based on the rate of \$13650/hectare and a replacement ratio of 3:1.

E17.0 Signs Code

Clause E17.7.1 - Standards for Signs

Acceptable Solution A1
A sign must comply with the standards listed in Table E.17.2 and be a permitted sign in Table E17.3.
Performance Criteria P1
A sign not complying with the standards in Table E17.2 or has discretionary status in Table E17.3 must satisfy all of the following:

- (a) be integrated into the design of the premises and streetscape so as to be attractive and informative without dominating the building or streetscape;
- (b) be of appropriate dimensions so as not to dominate the streetscape or premises on which it is located;
- (c) be constructed of materials which are able to be maintained in a satisfactory manner at all times;
- (d) not result in loss of amenity to neighbouring properties;
- (e) not involve the repetition of messages or information on the same street frontage;
- (f) not contribute to or exacerbate visual clutter;
- (g) not cause a safety hazard.

Proposal

The proposal includes wall signs and above awning signs which do not comply with the standards listed in Table E.17.2 of the Scheme.

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The proposed signage would be integrated into the design of the buildings being wall and above awning signs. The signs would be for business identification purposes.
- The size of the signs would not dominate the streetscape or buildings. The signs are large, however, are an appropriate scale considering the scale and use of the buildings. It is noted that the tenancies would only been provided with a maximum of two (2) signs per tenancy to ensure there is no repetition of messages or visual clutter.
- The signage would not result in any loss of amenity to neighbouring properties given it would be static business identification signage that is not proposed to be illuminated and would be integrated into the building design.
- The signage would be constructed with painted steel which would be durable and easily maintained.
- The signage would not cause a safety hazard being fixed to the buildings and would not impact sight lines.

2.5 Public Consultation and Representations

The application was advertised in accordance with the requirements of s.57 of the *Land Use Planning and Approvals Act 1993* (from 16 November 2024 to 29 November 2024). Three (3) representations were received during the public exhibition period. The following issues were raised by the representors:

2.5.1 Impacts on adjoining school

A representation was received that raised concerns on the impact of the development on the adjoining school which is zoned Community Purpose Zone. The representation states the setback of 3 metres is insufficient, and will inevitably adversely impact the amenity of the school. At 12m the combined height of the retaining wall and building, built on a significantly upwards-sloping hill, the actual impact will be far more than a 12m edifice on a flat adjoining property. The representation notes that if the school property was zoned Residential, the setback required would be at least twice that proposed, at 6m and with the land zoned Community Purposes and used as a school, the actual usage is not that different from Residential.

The representation states there will be significant capacity to overlook into the school with accompanying loss of privacy, and the structure will have a major visual impact when viewed from the school. In addition, the visual impact of a 12 metre structure, uphill, for anyone walking or driving up Maranoa Road, will be huge.

The representation requests that if an increased setback is refused, that the sill height of the windows overlooking the school be raised to 1700mm above floor level.

Lastly, the representation raised concerns about construction along the boundary in the setback zone. During construction, the representor would like it ensured that:

- works do not overflow onto the school site;
- the final ground height at the boundary is not higher than the current level;
- the slope of the land above the boundary is no steeper than it currently is;
- stormwater is managed on the development site and does not flow onto the school site.

Response

The subject site is located in the General Business Zone which does not include any side setback standards for land that is not in a residential zone. Irrespective of this, the application proposes a setback of 3m from the side boundary that adjoins the school. The proposal complies with the Scheme in terms of the setback to the school boundary.

Regarding height, the proposal complies with the Scheme with a maximum height of 12m above natural ground level. In terms of visual impacts when viewed from the adjoining property, there are no relevant standards that consider this matter based on the zoning of the site.

Regarding overlooking, again there is no standards in the Scheme that deal with this matter due to the zoning of the site. The representation was forwarded to the applicant to consider any design changes, however, no changes to the design have been proposed.

In terms of the construction management, these matters would be largely dealt with through building approvals, however, a soil and water management plan has been requested to be submitted through a condition. The plans also do not propose any change in the ground level/slope at the boundary. The application does include a Stormwater Management Plan for the proposed development. The development is drained in accordance with the requirements of the Scheme including the retaining walls adjacent the northern boundary with the school site.

2.5.2 Access and traffic

A representation was received that stated whilst they have no issue with the warehouses or development itself, there are concerns with the access to the development through the Maranoa Road intersection. The representor stated the size of the current access barely copes with the amount of traffic it gets during busy hours of the day. With the expected increase of traffic, a widening and improving of the intersection on Maranoa Road is essential. The representor states separate turning lanes on Maranoa Road would be helpful to safeguard a meaningful traffic flow on the road. The representor states once

you have turned onto the driveway of Kingston Town shopping centre property there is more cause for traffic to be blocked at the T-junction just inside the property. This traffic gets banked up and creates chaos on Maranoa Road - especially during high demand times and that this should also be remedied. A new intersection that allows improved traffic flow in and out and considers pedestrians' needs is essential in order to avoid frustrations and accidents in the future. This issue of access has not been addressed properly on the current plans but should not be ignored.

Response

The application was supported by a detailed TIA for the development and a capacity analysis for the intersections on Maranoa Road and Channel Highway. The traffic generation was modelled and demonstrated that it will not have a significant adverse impact on the traffic efficiency and general operation of the road network. The Maranoa Road intersection operates at Level of Service (LOS) A or B for all approaches. This LOS is maintained even with the development. The TIA does not raise any concerns with the Maranoa Road intersection or make any recommendations for upgrade of the intersection.

2.5.3 Impact on streetscape

A representation was received that raised concerns over the impact of the development on the Maranoa Road streetscape. The representation states the two showrooms would severely break the visual continuity of the streetscape and would effectively remove the greater part of the natural barrier frontage and replace it with minimal depth and height landscaping of alien culture. The representation states the design will dominate the streetscape and the picture frame sized windows facing Maranoa Road present ample opportunity for subliminal advertising messages through internal displays. This is different to the current shopping centre signage, which is discreetly displayed in the entrance, which is the local's entrance.

The representation states that the streetscape will be broken and the residential area visually connected into the shopping centre. The representation states the planning report mentions other land available for future development and that if this occurs, the site will take on a commercial dominance that would remove the residential nature to satisfy commercial aspirations.

Response

The site is zoned General Business Zone which allows and encourages commercial development. It is acknowledged that the proposal would be a substantial change to the streetscape as the area proposed to be developed is currently vacant and largely vegetated. However, the site is zoned to enable commercial development to occur on the site due to its proximity to the Kingston CBD. The proposal includes a discretion to allow a greater setback from the frontage to ensure the design is compatible and can integrate into the streetscape whilst still meeting the standards of the Scheme. This discretion enables landscaping to be proposed within the front setback. The large windows are also proposed to comply with the Scheme which requires large areas of glazing for passive surveillance.

2.5.4 Future uses at the shopping centre

A representation was received that raised concerns over the future use of the site due to the zoning of the site as General Business Zone. The representation states that the type of zoning allows for and is likely to attract businesses that are more commercial in nature and if approved has the potential to inflict serious deterioration of good existing town planning infrastructure.

Response

The zoning of the site is not something that can be addressed through this application. Council must assess the application based on the Scheme in effect at the time the application was made valid.

2.6 Other MattersTitle restrictions

The site is subject to the following title restrictions:

- SP182287 EASEMENTS in Schedule of Easements
- SP182287 COVENANTS in Schedule of Easements
- SP182287 FENCING PROVISION in Schedule of Easements
- E160163 BENEFITING EASEMENT: a Water Supply Easement (appurtenant to the land marked ABCDE on Sealed Plan 177320) over the land marked Private Service Easement to Supply Water 2.00 wide on Plan 181418
- E160163 BENEFITING EASEMENT: a Services Easement (appurtenant to the land marked ABCDE on Sealed Plan 177320) over the land marked Private Service Easement 1.00 wide on Plan 181418 Registered 13-Aug-2021 at noon
- SP 7485 FENCING PROVISION in Schedule of Easements

Comment:

The easements and fencing provisions have no impact on the proposal, however, the covenant listed in SP182287 does have implications for the proposal. The covenant listed below was put on the title as part of subdivision permit DAS-2020-16. This permit approved the subdivision of the land into two (2) lots to enable the land to the northwest of the existing lot to be subdivided for multiple dwellings (figure 12). Condition 7 of the permit required a covenant to be placed on the title for lot 2 restricting future development to multiple dwellings only. Lot 2 on the approved plans was the residential lot, however, when the final plan was lodged with Council the lot containing Kingston Town Shopping Centre was labelled as lot 2. The covenant refers to lot 2 as per the condition, however, it should have referred to lot 1. Accordingly, a condition has been recommended that the covenant must be amended prior to commencement of works.

COVENANTS:

The owner of lot 2 on the Plan covenants with the Kingborough Council to the intent that the burden of this covenant may run with and bind the covenantor's lot and each and every part of it and that the benefit of the covenant shall be annexed to and devolve with Kingborough Council to observe the following stipulation-

1. Not to develop the lot except for multiple dwellings only

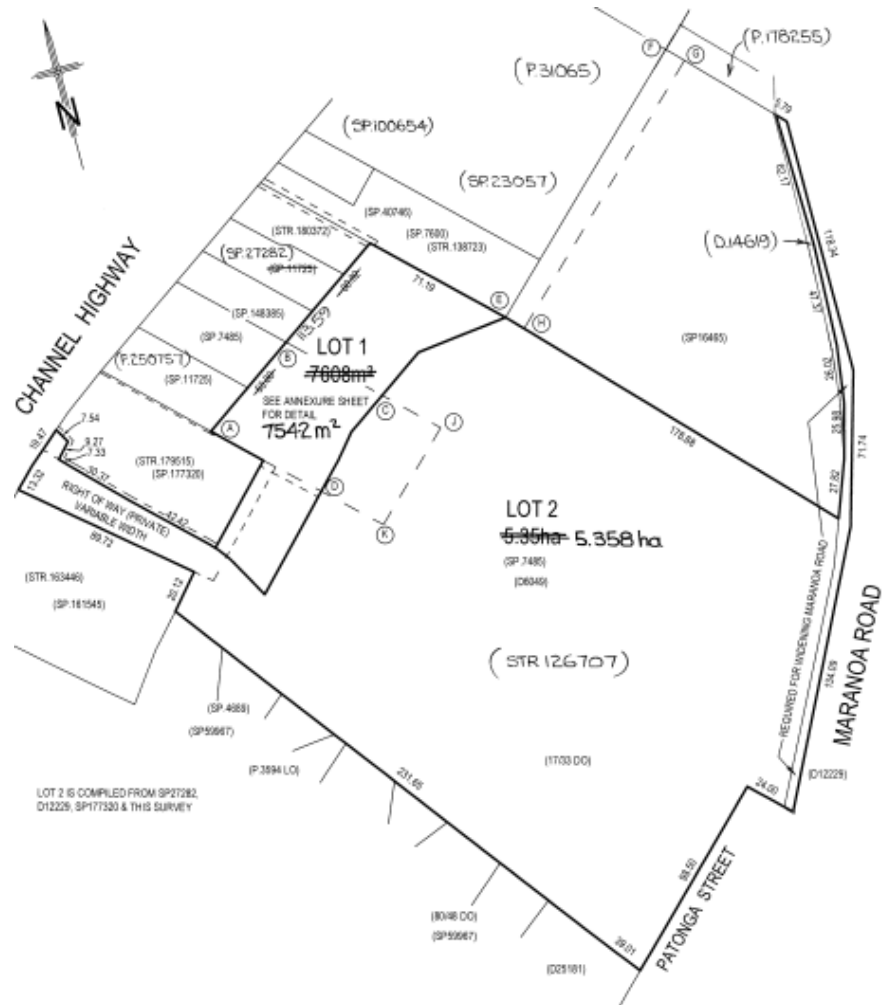


Figure 12: Plan contained in SP182287

TasWater

TasWater did not object to the proposal and have provided a Submission to Planning Authority Notice.

TasNetworks

TasNetworks did not object to the application or require any conditions to be included in any approval. Only advice was provided which was provided to the application.

Extension of time to make a decision

The applicant granted an extension of time for the Planning Authority to make a decision (s57(6A) of the Land Use Planning and Approvals Act 1993) until 21 January 2025 therefore allowing the report to be considered at a Council meeting, as required by delegations where there are three or more opposing representations.

3. CONCLUSION

The proposal is relying on the performance criteria to comply with applicable standards. The proposal is assessed as satisfying the performance criteria and complies with those standards. The proposal is assessed as complying with all other use and development standards in the General Business Zone, as well as the applicable standards of the relevant codes. The application was publicly advertised for the statutory 14-day period and three (3) representations were received which have been addressed in this report. It is concluded that the proposal is consistent with the Scheme’s zone purpose statements and is satisfactory.

4. RECOMMENDATION

That the seven (7) warehouses, two (2) showrooms and associated works at 'Kingston Town', 37-59 Maranoa Road, Kingston and adjoining Council Road Reserve be approved subject to the following conditions:

1. Prior to the commencement of works, amended plans to the satisfaction and approval of the Manager Development Services must be submitted to and approved. When approved, the plans will be endorsed and will then form part of this permit. The plans must be generally in accordance with Council Plan Reference No. P3 submitted on 27 September 2024 but modified to show:
 - (a) Variation to the finish/colours/materials of the lower-level eastern elevation (facing Maranoa Road) to provide articulation and interest to break up the large expanse of blank walls (to replace plans A10.0, A11.0, A15.0, A16.0, A19.0);
 - (b) Location of bicycle parking for customers and employees in accordance with Table E6.2 of the KIPS 2015 as follows:
 - a. Customers; minimum of 2 spaces – Class 3 (lockable hoops).
 - b. Employees; minimum of 2 spaces – Class 1/2 (Enclosed lockers or locked compound);
 - (c) Location of 4 motorcycle parking spaces;
 - (d) Location of rubbish storage and collection;
 - (e) Removal of parking calculation tables on plan A2.0;
 - (f) Removal of notation of 'bulky goods sales' for warehouse 1 on plan A3.0;
 - (g) Notation that the building surfaces facing the Community Purpose and General Residential Zone have a light reflectance value not greater than 40%; and
 - (h) Landscaping as per Condition 3 of this Permit.

Except as otherwise required by this Permit, use and development of the land must be substantially in accordance with Development Application No. DA-2024-239 and the endorsed plans.

This Permit relates to the use of land or buildings irrespective of the applicant or subsequent occupants, and whoever acts on it must comply with all conditions in this Permit. Any amendment, variation or extension of this Permit requires further planning consent of Council.

2. Prior to the commencement of works, the covenant contained in SP182287 must be amended via a petition to amend a sealed plan under Section 103 of the *Local Government (Building and Miscellaneous Provisions) Act 1993* to enable development of the land for development other than multiple dwellings by deleting reference to lot 2 and replacing with lot 1.
3. Prior to the commencement of works, landscaping plans must be submitted for approval by Council's Manager Development Services. The landscape plan must be prepared by a suitably qualified person and be at a suitable scale, and indicate the following:
 - (a) outline of the proposed buildings and hard stand areas;
 - (b) proposed planting by quantity, genus, species, common name, expected mature height and plant size.

- (c) Landscaping must be proposed within the landscape areas indicated on the plans, including around the parking areas. Particular focus should be on proposed landscaping to the east of the building (addressing Maranoa Road) to reduce the scale and bulk of the building (therefore using vegetation of various size including larger vegetation to a mature height of at least 6m;
- (d) existing trees to be retained and proposed measures to be carried out for their preparation and protection during construction;
- (e) earth shaping proposals, including retaining wall(s);
- (f) fencing, paths and paving (indicating materials and surface finish); and
- (g) proposed maintenance program.

The use of drought resistant and Tasmanian native species is encouraged.

Once endorsed the plans will form part of the permit. It is recommended that the consideration be given to Council's Landscape Guidelines (Preparing a Landscape Plan), which is available on Council's website.

4. Prior to commencement of on-site works associated with the private infrastructure to service the approved development, engineering design drawings must be submitted to Council for approval. The engineering plans and specifications must be prepared and certified by a professional Civil Engineer. Plans must be to satisfaction of the Director Engineering Services and comply with:
- Tasmanian Standard Drawings
 - Austroads Standards and Australian Standards
 - Australian Rainfall and Runoff Guidelines

The Plans must include, but are not limited to:

- A. Detailed internal vehicular and pedestrian access, carparking and manoeuvring areas as per the submitted Car Park Plan C-100 P4 by Flussig Engineers including:
- i. Longitudinal and Cross sections of the driveway/access road.
 - ii. Contours, finish levels and gradients of the driveway/access road.
 - iii. A minimum of 45 carparking spaces (including one accessible space).
 - iv. A minimum of 4 motorcycle parking spaces.
 - v. Wheel stops for open parking bays (as appropriate).
 - vi. Lighting for parking and vehicle circulation roadways and pedestrian paths.
 - vii. Surface treatment and stormwater drainage.
- B. Design (including supporting documentation and hydraulic calculations) of the proposed stormwater infrastructure including:
- i. Stormwater connection to service the development.
 - ii. A water sensitive urban design system to achieve the acceptable stormwater quality and quantity targets required in Table E7.1 of the Kingborough Interim Planning Scheme 2015 as per the submitted Stormwater Management Plan by Flussig Engineers, April 2024.

- iii. Overland flowpaths sized to accommodate the estimated 1% AEP (Annual Exceedance Probability) flow as per the submitted Flood Hazard Report by Flussig Engineers, July 2024.
- C. A Tree Plan consistent with the Natural Values Assessment (NVA) (Enviro-dynamics, V4, December 2024):
- i. identifying all individual native trees and their associated tree protection zones relative to buildings and works;
 - ii. specifying trees for removal and retention;
 - iii. demonstrating the extent of impacts on trees for retention are no greater than shown in the endorsed plans and Natural Values Assessment (NVA) (Enviro-dynamics, V4, December 2024); and
 - iv. identifying the location of tree protection fencing consistent with Conditions 15 and 16.

Once endorsed the plans will form part of the permit.

5. Prior to works commencing (including demolition and excavation), a Construction Management Plan (CMP) must be submitted to and endorsed by the Manager Development Services. The plan must include (but not limited to):
- (a) Hours for construction activity in accordance with any other condition of this Permit;
 - (b) Measures to control noise and dust;
 - (c) A Soil and Water Management Plan;
 - (d) Storage locations for the stockpiling of fill on site including demonstration that the stockpiling of fill will not encroach into the Tree Root Protection Zones of trees identified for retention;
 - (e) The location for the disposal of any excess fill off site and demonstration this site is either a certified landfill facility for Level 1 fill or a site that has been approved for the disposal or use of Level 1 fill under a development use permit issued by Council;
 - (f) The specifications and location of tree protection measures in accordance with Condition 16;
 - (g) A plan showing the location of parking areas for construction and sub-contractors' vehicles on and surrounding the site, to ensure that vehicles associated with construction activity cause minimum disruption to surrounding premises.
 - (h) A plan showing the location and design of a vehicle wash-down bay for construction vehicles on the site;
 - (i) Measures to ensure that sub-contractors / tradespersons operating on the site are aware of the contents of the Construction Management Plan;
 - (j) Contact details of key construction site staff;
 - (k) A site plan showing the location of any site sheds, on-site amenities, building waste storage and the like, noting that Council does not support site sheds on Council road reserves.

A site inspection of the implemented plan by the Council's Development Inspector and Council's Environmental Planner must be satisfactorily undertaken with the principal contractor prior to the commencement of any on-site works and issue of start of works.

Once endorsed, the Plan will form part of the permit and must be implemented and maintained throughout construction of the development to the satisfaction of the Manager Development Services.

6. Unless by way of separate approval, the use of the tenancies are limited to the following uses classes (as defined by the Kingborough Interim Scheme 2015):

- (a) Warehouses 1-7 – 'storage' use class
- (b) Showrooms 1 and 2 – 'bulky goods sales' use class

Note: the mezzanine area in Warehouse 7 cannot be converted to office space (or other use other than 'storage') without further approval.

7. The uses must only operate between 7.00am – 7.00pm 7 days a week (including public holidays).
8. The area set aside for parking of vehicles and so delineated on the approved engineering design drawings must be made available for such use and must not be used for any other purpose.
9. Commercial vehicle movements (including loading and unloading and garbage removal) to or from the site must be within the hours of 6.00 am to 10.00 pm Monday to Saturday.
10. External lighting must comply with all of the following:
- (a) Be turned off between 11.00pm and 6.00am, except for security lighting.
 - (b) Security lighting must be baffled to ensure that it does not cause emission of light outside the property.
11. Noise emissions measured at the boundary of a residential zone must not exceed the following:
- (a) 55dB(A) (LAeq) between the hours of 7.00 am to 7.00 pm;
 - (b) 5dB(A) above the background (LA90) level or 40dB(A) (LAeq), whichever is the lower, between the hours of 7.00 pm to 7.00 am;
 - (c) 65dB(A) (LAm_{ax}) at any time.

Measurement of noise levels must be in accordance with the methods in the Tasmanian Noise Measurement Procedures Manual, second edition, July 2008, issued by the Director of Environmental Management, including adjustment of noise levels for tonality and impulsiveness. Noise levels are to be averaged over a 15 minute time interval.

12. The loading and unloading of goods from vehicles must only be carried out on the land (within the designated loading bays) and not on any part of any road reserve or public open space. Loading and unloading must not disrupt the circulation and parking of vehicles on the land, to the satisfaction of the Manager Development Services.

13. Approved native tree/vegetation removal and modification:

- (i) Is limited to:

- (a) seven (7) trees (Trees 3-7 and 12-13) and as shown in Council Plan Reference P3 (submitted on 27 September 2024) and Natural Values Assessment (Enviro-dynamics, V4, December 2024); and
 - (b) no more than 50m² of Gahnia radula within the footprint of Showroom 1, and as shown in Figure 9 of the Natural Values Assessment (Enviro-dynamics, V4, December 2024).
- (ii) Must not occur prior to building approval, securing the conservation offset and approval of an 'Application for Approval of Planning Start of Works Notice'.

No further felling, lopping, ringbarking or otherwise injuring or destroying of native vegetation or individual trees is approved as part of this planning permit.

14. Prior to the removal of any native vegetation and the commencement of on-site works, the loss of 50m² of potential Chaostola skipper habitat and removal of four (4) trees of very high conservation value must be offset by a financial contribution totalling \$2,484.75. This contribution must be paid to Council's Environmental Fund and used solely for the protection and management of Chaostola skipper and hollow dwelling habitat in the vicinity of Kingston or an area that supports equivalent biodiversity values within the municipality.

For Advice: In the event that a subdivision permit is issued for DAS-2024-9 and this permit includes an offset payment, payment of the offset for the subdivision is to be deducted from the offset required under this condition.

15. All remaining native trees identified for retention in Council Plan Reference P3 (submitted on 27 September 2024) and Natural Values Assessment (Enviro-dynamics, V4, December 2024) must be appropriately protected during and after construction. This includes but is not limited to implementation of the following measures:

A. Prior to Construction:

Prior to the commencement of any on-site works (including but not limited to vegetation removal, demolition, excavations, placement of fill, delivery of building/construction materials and/or temporary buildings):

- a) Installing tree protection fencing in accordance with Condition 16.
- b) Providing evidence of satisfactory installation of this fencing to the Manager Development Services prior to the commencement of any on-site works.

B. During Construction:

Maintaining tree protection measures required above for the duration of the construction.

C. Post Construction:

Adhering to the following tree management measures post construction for all areas within the tree protection zone but outside the footprint of the approved works:

- a) the existing soil level must not be altered around the tree protection zone of the trees (including the disposal of fill, placement of materials or the scalping of the soil);
- b) the tree protection zone must be free from the storage of fill, contaminants or other materials;

- c) machinery and vehicles are not permitted to access the tree protection zone; and
 - d) development and associated works are not permitted unless otherwise approved by Council in writing or otherwise in accordance with the law.
16. Tree protection fencing required under Condition 15 must:
- (a) Be located on the edge of the tree protection zone, unless the outer edge of works as shown on the endorsed plan are closer, in which case this tree/vegetation protection fencing may be reduced to the minimum amount necessary to allow the works to be completed.
 - (b) Exclude the following from the tree protection zones:
 - (a) Machine excavation including trenching.
 - (b) Machinery movement.
 - (c) Excavation of silt fencing.
 - (d) Cultivation.
 - (e) Storage.
 - (f) Preparation of chemicals, including preparation of cement products.
 - (g) Parking of vehicles and plant.
 - (h) Refuelling.
 - (i) Dumping waste.
 - (j) Placement of fill.
 - (k) Lighting of fires.
 - (l) Soil level changes.
 - (m) Temporary or permanent installation of utilities and signs.
 - (n) Physical damage to the trees.
 - (c) Be constructed in accordance with the following requirements:
 - (a) Utilise barrier mesh and star pickets fencing unless otherwise approved in writing.
 - (b) Form a visual and physical barrier.
 - (c) Be a minimum height of 1.5 metres above ground level.
 - (d) Include signage clearly marked "Tree Protection Zone - No Entry" on all sides.
17. Prior to the commencement of on-site works, including vegetation removal or modification, demolition, construction, excavations, placement of fill, delivery of building/construction materials and/or temporary buildings, an 'Application for Approval of Planning Start of Works Notice' must be lodged with Council's Planning Department.

This application must be lodged a minimum of 14 days prior to commencement of on-site works and works must not commence until this notice has been approved by the

- Manager Development Services. A copy of the application form is available on Council's website.
18. The construction works must be undertaken in accordance with the approved engineering design drawings to the satisfaction and approval of the Director Engineering Services.
 19. To the satisfaction of the Manager Development Services, the approved signs must:
 - (a) Not contain any flashing or moving light; and
 - (b) Be constructed and maintained.
 20. Exterior building surfaces of walls facing a residential zone must be coloured using colours with a light reflectance value not greater than 40 percent. Plans submitted for building approval must include a full schedule of colours and materials that demonstrate compliance with this condition.
 21. Landscaping must be provided prior to the occupation of the new buildings to the satisfaction of the Council's Manager Development Services. The landscaping areas shown on the endorsed plans must be used for landscaping and no other purpose and any landscaping must be maintained to the satisfaction of the Manager Development Services, including that any dead, diseased or damaged plants are to be replaced.
 22. Prior to the commencement of the use, adequate provision must be made for the storage and collection of garbage and other solid wastes within the curtilage of the site. This area must be properly graded, drained, and screened from public view to the satisfaction of the Manager Development Services.
 23. Prior to the occupation of any tenancy the following works must be completed in accordance with the endorsed plans to the satisfaction of the Council:
 - The parking areas (including signage and access);
 - Drainage works undertaken and completed;
 - The required landscaping; and
 - Waste storage area completed.
 24. The conditions as determined by TasWater, and set out in the attached Appendix A, form part of this permit.

ADVICE

- A. In accordance with section 53(5) of the *Land Use Planning and Approvals Act 1993* this permit lapses after a period of two years from the date on which it is granted if the use or development in respect of which it is granted is not substantially commenced within that period.
- B. An application for Notifiable Plumbing Work must be lodged with Council before commencing any work.
- C. The approval in this permit is under the *Land Use Planning and Approvals Act 1993* and does not provide any approvals under other Acts including, but not limited to *Building Act 2016*, *Urban Drainage Act 2013*, *Food Act 2003* or Council by-laws.

If your development involves demolition, new buildings or alterations to buildings (including plumbing works or onsite wastewater treatment) it is likely that you will be required to get approvals under the *Building Act 2016*. Change of use, including visitor

accommodation, may also require approval under the *Building Act 2016*. Advice should be sought from Council's Building Department or an independent building surveyor to establish any requirements.

- D. A drainage design plan at a scale of 1:200, designed by a qualified Hydraulic Designer, showing the location of the proposed sewer and stormwater house connection drains; including the pipe sizes, pits and driveway drainage, must be submitted with the application for Plumbing Permit.
- E. Prior to commencement of any works or the connection of any utility services within a Council road reservation, a 'Road Works Permit' is required to be approved and issued. An application can be made on Council's website as per the link, [Road Works and Occupation Permits](#).

ATTACHMENTS

- 1. Assessment Checklist**
- 2. Application Plans**
- 3. Certificate of Title**
- 4. TasWater Submission**

Public Copy

Assessment Checklist for Development Applications for Use and/or Development within the General Business Zone

Application No: DA-2024-239	Description: Seven (7) warehouses, two (2) showrooms and 48 carparking spaces
Applicant: Matt Kennedy Drafting & Design	Owner: Zacharia Investments Pty Ltd
Location: 'Kingston Town', 37-59 Maranoa Road, Kingston	

Use Status


Use Class	Storage and Bulky Goods Sales
Use Status	Discretionary

General Business Zone Provisions (use and/or development)

Checklist is based on KIPS2015 and provisions of PD8 (which commenced 22 Feb 2022)

Clause	Compliance/Comments
21.3 Use Standards	
<p>Clause 21.3.1 – Hours of Operation</p> <p>A1 – Hours of operation of a use within 50 m of a residential zone must be within:</p> <p>(a) 6.00 am to 10.00 pm Mondays to Saturdays inclusive;</p> <p>(b) 7.00 am to 9.00 pm Sundays and Public Holidays.</p> <p>except for office and administrative tasks.</p>	<p>A1 – Complies.</p> <p>The site is located within 50m of land zoned General Residential and Inner Residential therefore this standard is applicable.</p> <p>The uses are proposed to operate 7.00am – 7.00pm 7 days a week including public holidays.</p> <p>A condition has been recommended to be included on the permit to ensure ongoing compliance.</p>
<p>Clause 21.3.2 – Noise</p> <p>A1 – Noise emissions measured at the boundary of a residential zone must not exceed the following:</p> <p>(a) 55dB(A) (LAeq) between the hours of 7.00 am to 7.00 pm;</p> <p>(b) 5dB(A) above the background (LA90) level or 40dB(A) (LAeq), whichever is the lower, between the hours of 7.00 pm to 7.00 am;</p> <p>(c) 65dB(A) (LAm_{ax}) at any time.</p> <p>Measurement of noise levels must be in accordance with the methods in the Tasmanian Noise Measurement Procedures Manual, second edition, July 2008, issued by the Director of Environmental Management, including adjustment of noise levels for tonality and impulsiveness.</p> <p>Noise levels are to be averaged over a 15 minute time interval.</p>	<p>A1 – Complies.</p> <p>The application was supported by a noise assessment prepared by Noise Vibration Consulting that demonstrates the uses would comply with A1.</p> <p>A condition has been recommended to be included on the permit to ensure ongoing compliance.</p>

Clause	Compliance/Comments
<p>Clause 21.3.3 – External Lighting A1 – External lighting within 50 m of a residential zone must comply with all of the following: (a) be turned off between 11:00 pm and 6:00 am, except for security lighting; (b) security lighting must be baffled to ensure they do not cause emission of light outside the zone.</p>	<p>A1 – Complies. The lighting on the eastern side of the buildings would be within 50m of a residential zone. The lighting would be turned off between 11:00 pm and 6:00 am, except for security lighting. The security lighting would be baffled to ensure they do not cause emission of light outside the zone. A condition has been recommended to be included on the permit to ensure ongoing compliance.</p>
<p>Clause 21.3.4 – Commercial Vehicle Movements A1 – Commercial vehicle movements, (including loading and unloading and garbage removal) to or from a site within 50 m of a residential zone must be within the hours of: (a) 6.00 am to 10.00 pm Mondays to Saturdays inclusive; (b) 7.00 am to 9.00 pm Sundays and public holidays.</p>	<p>A1 – Complies. The site is located within 50m of land zoned General Residential and Inner Residential therefore this standard is applicable. The commercial vehicle movements associated with the uses are proposed between the hours of 6.00am and 10.00pm Monday to Saturday. A condition has been recommended to be included on the permit to ensure ongoing compliance.</p>
<p>21.4 Development Standards for Buildings and Works</p>	
<p>Clause 20.4.1 – Building Height A1 – Building height must be no more than: 12 m.</p>	<p>A1 – Complies. The building would have a maximum height above NGL of 12m.</p>
<p>A2 – Building height within 10 m of a residential zone must be no more than 8.5 m.</p>	<p>A2 – N/A The buildings are not located within 10m of a residential zone.</p>
<p>Clause 21.4.2 – Setback A1 – Building setback from frontage must be parallel to the frontage and must be no more than: 5 m, if fronting Channel Highway. 3 m, if fronting any other street. A2 – Building setback from a residential zone must be no less than: (a) 5 m; (b) half the height of the wall, whichever is the greater.</p>	<p>A1 – Does not comply. The buildings are proposed to be setback between 3m and 5m from the Maranoa Road frontage. This is more than the 3m permitted under the AS. A2 – Complies. The nearest residential zone is the General Residential Zone (red) that is located to the southeast. The buildings would be setback approximately 9m which is more than half the height of the wall as required by the Acceptable Solution.</p>

Clause	Compliance/Comments
	
<p>Clause 21.4.3 – Design</p> <p>A1 – Building design must comply with all of the following:</p> <p>(a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;</p> <p>(b) for new building or alterations to an existing facade provide windows and door openings at ground floor level in the front façade no less than 40% of the surface area of the ground floor level façade;</p> <p>(c) for new building or alterations to an existing facade ensure any single expanse of blank wall in the ground level front façade and facades facing other public spaces is not greater than 30% of the length of the facade;</p> <p>(d) screen mechanical plant and miscellaneous equipment such as heat pumps, air conditioning units, switchboards, hot water units or similar from view from the street and other public spaces;</p> <p>(e) incorporate roof-top service infrastructure, including service plants and lift structures, within the design of the roof;</p> <p>(f) provide awnings over the public footpath if existing on the site or on adjoining lots;</p> <p>(g) not include security shutters over windows or doors with a frontage to a street or public place.</p>	<p>A1 – Does not comply.</p> <p>A1(a) – Complies. Pedestrian entrance to the showrooms would be visible from the street and the publicly accessible car parking area in the site. Pedestrian entrance to the warehouses would be visible from the publicly accessible car parking area in the site.</p> <p>A1(b) – Does not comply. Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the Acceptable Solution as a large retaining structure is proposed at ground level.</p> <p>A1(c) – Does not comply. Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the Acceptable Solution as a large retaining structure is proposed at ground level.</p> <p>A1(d) – Complies.</p> <p>A1(e) – Complies.</p> <p>A1(f) – N/A No awnings over public footpaths on the site or on adjoining lots.</p> <p>A1(g) – Complies.</p>
<p>A2 – Walls of a building facing a residential zone must be coloured using colours with a light reflectance value not greater than 40 percent.</p>	<p>A2 – Complies.</p> <p>The buildings are proposed to be finished with natural coloured precast concrete, dark grey painted finish panels and shale grey trimmings and garage doors. The Planning Report notes all the external colours have a LRV of less than 40%, however, shale grey is understood to have a LRV of 49%. To ensure compliance, a condition has been recommended requiring the wall to have a light reflectance value not greater than 40%.</p>

Clause	Compliance/Comments
<p>Clause 21.4.4 – Passive Surveillance A1 – Building design must comply with all of the following:</p> <p>(a) provide the main pedestrian entrance to the building so that it is clearly visible from the road or publicly accessible areas on the site;</p> <p>(b) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the front façade which amount to no less than 40 % of the surface area of the ground floor level facade;</p> <p>(c) for new buildings or alterations to an existing facade provide windows and door openings at ground floor level in the façade of any wall which faces a public space or a car park which amount to no less than 30 % of the surface area of the ground floor level facade;</p> <p>(d) avoid creating entrapment spaces around the building site, such as concealed alcoves near public spaces;</p> <p>(e) provide external lighting to illuminate car parking areas and pathways;</p> <p>(f) provide well-lit public access at the ground floor level from any external car park.</p>	<p>A1 – Does not comply. A1(a) – Complies. Pedestrian entrance to the showrooms would be visible from the street and the publicly accessible car parking area in the site. Pedestrian entrance to the warehouses would be visible from the publicly accessible car parking area in the site. A1(b) – Does not comply. Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the Acceptable Solution as a large retaining structure is proposed at ground level. A1(c) – Does not comply. Due to the slope of the land, the ground level front façade of the buildings facing Maranoa Road do not comply with the Acceptable Solution as a large retaining structure is proposed at ground level. A1(d) – Complies. No entrapment spaces proposed. A1(e) – Complies. A1(f) – Complies.</p>
<p>Clause 21.4.5 – Landscaping A1 – Landscaping along the frontage of a site is not required if all of the following apply:</p> <p>(a) the building extends across the width of the frontage, (except for vehicular access ways);</p> <p>(b) the building has a setback from the frontage of no more than 1m.</p>	<p>A1 – Complies. As the building is setback more than 1m from the frontage, landscaping has been provided along the Maranoa Road frontage. A condition requiring a detailed landscaping plan has been recommended.</p>
<p>A2 – Along a boundary with a residential zone landscaping must be provided for a depth no less than: 2 m.</p>	<p>A2 – N/A The location of the development is not on an area of the site that adjoins a residential zone (residential zone is within 50m but not adjoining the eastern side of the site) and the portion of the site that does adjoin a residential zone is developed with existing landscaping.</p>
<p>Clause 21.4.6 – Outdoor Storage Areas A1 – Outdoor storage areas for non-residential uses must comply with all of the following:</p> <p>(a) be located behind the building line;</p> <p>(b) all goods and materials stored must be screened from public view;</p> <p>(c) not encroach upon car parking areas, driveways or landscaped areas.</p>	<p>A1 – N/A No outdoor storage areas are proposed.</p>
<p>Clause 21.4.7 – Fencing A1 – Fencing must comply with all of the following:</p> <p>(a) fences, walls and gates of greater height than 1.5 m must not be erected within 4.5 m of the frontage;</p> <p>(b) fences along a frontage must be at least 50% transparent above a height of 1.2 m;</p>	<p>A1 – N/A None proposed.</p>

Clause	Compliance/Comments
(c) height of fences along a common boundary with land in a residential zone must be no more than 2.1 m and must not contain barbed wire.	
<p>Clause 21.4.8 – Environmental Values A1 – No trees of high conservation value will be impacted.</p>	<p>A1 – Does not comply. A Natural Values Assessment (NVA) (Environmental Dynamics, V4, December 2024) submitted in response to a further information request confirms that there are four (4) trees of very high conservation on the subject land, all of which are proposed for removal (Trees 5, 6, 7 and 13) for the proposed show rooms and associated infrastructure. The proposal also includes the removal of a further three native trees (Trees 3, 4 and 12). However, these trees are not of high conservation value. An arborist assessment (Tree Glider, 21 September 2024) submitted in response to a further information request confirms that trees located on adjacent land but in proximity to the proposed buildings and works are proposed for and capable of retention.</p>

Code Provisions

Clause	Compliance/Comments
E2.0 Potentially Contaminated Land Code	
<p>Clause E2.6.1 – Subdivision A1 – For subdivision of land, the Director, or a person approved by the Director for the purpose of this Code: (a) certifies that the land is suitable for the intended use; or (b) approves a plan to manage contamination and associated risk to human health or the environment, that will ensure the subdivision does not adversely impact on health or the environment and is suitable for its intended use.</p>	A1 – N/A
<p>Clause E2.6.2 - Excavation A1 – No acceptable solution (requires assessment against performance criteria)</p>	<p>A1 – Does not comply. No Acceptable Solution.</p>
E3.0 Landslide Code	
<p>The subject site is affected by this Code; however, it does not occur on the parts of the site that are proposed for buildings or works, and the use is neither vulnerable nor hazardous. Therefore, an assessment against the code is not required.</p>	
E5.0 Road and Railway Assets Code	
<p>Clause E5.5.1 – Existing road accesses and junctions A1 – The annual average daily traffic (AADT) of vehicle movements, to and from a site, onto a category 1 or category 2 road, in an area subject to a speed limit of more than 60km/h, must not increase by more than 10% or 10 vehicle movements per day, whichever is the greater.</p>	A1 – N/A

Clause	Compliance/Comments
<p>A2 – The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of more than 60km/h, must not increase by more than 10% or 10 vehicle movements per day, whichever is the greater.</p>	<p>A2 – N/A</p>
<p>A3 – The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.</p>	<p>A3 – Does not comply. The peak traffic generation would be 34 vehicles per hour which would equate to 17 inward vehicles per hour and 17 outward vehicles per hour across two accesses. This exceeds the 40 vehicle movements per day allowed under the Acceptable Solution.</p>
<p>E5.5.2 Exiting level crossings A1 – Where use has access across part of a rail network, the annual average daily traffic (AADT) at an existing level crossing must not be increased by greater than 10% or 10 vehicle movements per day, whichever is the greater.</p>	<p>A1 – N/A</p>
<p>Clause E5.6.1 - Development adjacent to roads and railways A1.1 – Except as provided in A1.2, the following development must be located at least 50m from the rail network, or a category 1 road or category 2 road, in an area subject to a speed limit of more than 60km/h: (a) new buildings; (b) other road or earth works; and (c) building envelopes on new lots.</p>	<p>A1.1 – N/A Site is not adjoining a category 1 or 2 road.</p>
<p>A1.2 – Buildings, may be: (a) located within a row of existing buildings and setback no closer than the immediately adjacent building; or (b) an extension which extends no closer than: (i) the existing building; or (ii) an immediately adjacent building.</p>	<p>A1.2 – N/A</p>
<p>Clause E5.6.2 - Road access and junctions A1 – No new access or junction to roads in an area subject to a speed limit of more than 60km/h.</p>	<p>A1 – N/A</p>
<p>A2 – No more than one access providing both entry and exit, or two accesses providing separate entry and exit, to roads in an area subject to a speed limit of 60km/h or less.</p>	<p>A2 – N/A No new accesses are proposed.</p>
<p>Clause E5.6.3 – New level crossings A1 – No acceptable solution (requires assessment against performance criteria)</p>	<p>A1 – N/A</p>
<p>Clause E5.6.4 - Sight distance at accesses, junctions and level crossings A1 – Sight distances at: (a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; and</p>	<p>A1 – Complies.</p>

Clause	Compliance/Comments
(b) rail level crossings must comply with AS1742.7 Manual of uniform traffic control devices - Railway crossings, Standards Association of Australia.	
E6.0 Parking and Access Code	
<p>Clause E6.6.1 - Number of car parking spaces</p> <p>A1 - The number of on-site car parking spaces must be:</p> <p>(a) no less than the number specified in Table E6.1;</p> <p>except if:</p> <p>(i) the site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;</p>	<p>A1 – Complies.</p> <p>A total of 45 spaces is required in accordance with the planning scheme. This is based on a use combination of Bulky Goods (1 per 50m²) or Storage (1 per 100m²) and Ancillary office (1 per 40m²). Refer Drawing A2.0.</p> <ul style="list-style-type: none"> • Warehouse 1 requires 4 carparking spaces. • Warehouse 2 – 6 requires 18 carparking spaces. • Warehouse 7 requires 4 carparking spaces. • Showrooms 1 and 2 require 19 carparking spaces. <p>Please refer to report for table of full calculations. 46 spaces and one access space are indicated on the drawings.</p> <p>It is noted that the TIA submitted with the application notes that 51 spaces are required, however, this is incorrect as per the above table.</p>
<p>Clause E6.6.2 – Number of Accessible Car Parking Spaces for People with a Disability</p> <p>A1 - Car parking spaces provided for people with a disability must:</p> <p>(a) satisfy the relevant provisions of the Building Code of Australia;</p> <p>(b) be incorporated into the overall car park design;</p> <p>(c) be located as close as practicable to the building entrance.</p>	<p>A1 – Complies.</p> <p>One (1) access space is to be provided as per the submitted plans in compliance with the planning scheme</p>
<p>Clause E6.6.3 – Number of Motorcycle Parking Spaces</p> <p>A1 - The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced.</p>	<p>A1 – Complies.</p> <p>Based on the number of required spaces (44), a total of 3 motorcycle spaces is required. The 2 excess car parking spaces will allow for 4 motorcycle spaces. It is recommended the permit be conditioned for 4 motorcycle spaces to be provided in lieu of the 2 excess car spaces.</p>
<p>Clause E6.6.4 – Number of Bicycle Parking Spaces</p> <p>A1 - The number of on-site bicycle parking spaces provided must be no less than the number specified in Table E6.2.</p>	<p>A1 – Complies.</p> <p>Total site bulky goods use requires 2 employee spaces – class 1 or 2. Customers (bulky goods) require a minimum of 2 spaces – class 3. No requirement for storage use component.</p> <p>It is recommended this permit be conditioned for 2 employee bicycle spaces to be provided – class 1/2. Also 2 customer bicycle spaces to be provided – class 3.</p>

Clause	Compliance/Comments
<p>Clause E6.7.1 - Number of vehicular accesses A1 – The number of vehicle access points provided for each road frontage must be no more than 1 or the existing number of vehicle access points, whichever is the greater.</p>	<p>A1 – Complies. Existing access.</p>
<p>Clause E6.7.2 - Design of vehicular accesses A1 – Design of vehicle access points must comply with all of the following: (a) in the case of non-commercial vehicle access; the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 – “Access Facilities to Off-street Parking Areas and Queuing Areas” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking; (b) in the case of commercial vehicle access; the location, sight distance, geometry and gradient of an access must be designed and constructed to comply with all access driveway provisions in section 3 “Access Driveways and Circulation Roadways” of AS2890.2 - 2002 Parking facilities Part 2: Off-street commercial vehicle facilities.</p>	<p>A1 – Complies.</p>
<p>Clause E6.7.3 - Vehicular passing areas along an access A1 – Vehicular passing areas must: (a) be provided if any of the following applies to an access: (i) it serves more than 5 car parking spaces; (ii) is more than 30 m long; (iii) it meets a road serving more than 6000 vehicles per day (b) be 6 m long, 5.5 m wide, and taper to the width of the driveway; (c) it meets a road serving more than 6000 vehicles per day; (d) have the first passing area constructed at the kerb; (e) be at intervals of no more than 30 m along the access.</p>	<p>A1 – Complies. The standard isle width between the parking modules is provided as required by the AS.</p>
<p>Clause E6.7.4 - On-site turning A1 – On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following: (a) it serves no more than two dwelling units;</p>	<p>A1 – Complies. The standard isle width between the parking modules is provided as required by the AS.</p>
<p>Clause E6.7.5 - Layout of parking areas A1 – The layout of car parking spaces, access aisles, circulation roadways and ramps must be designed and constructed to comply with section 2 “Design of Parking Modules, Circulation Roadways and Ramps” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking and must have sufficient headroom to comply with clause 5.3 “Headroom” of the same Standard.</p>	<p>A1 – Complies. The standard dimensions of parking modules and bays is provided as required by the AS2890.</p>

Clause	Compliance/Comments
<p>Clause E6.7.6 - Surface treatment of parking areas</p> <p>A1 – Parking spaces and vehicle circulation roadways must be in accordance with all of the following;</p> <p>(a) paved or treated with a durable all-weather pavement where within 75m of a property boundary or a sealed roadway;</p> <p>(b) drained to an approved stormwater system, unless the road from which access is provided to the property is unsealed.</p>	<p>A1 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>Clause E6.7.7 - Lighting of parking areas</p> <p>A1 – Parking and vehicle circulation roadways and pedestrian paths serving 5 or more car parking spaces, used outside daylight hours, must be provided with lighting in accordance with clause 3.1 “Basis of Design” and clause 3.6 “Car Parks” in AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting.</p>	<p>A1 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>Clause E6.7.8 - Landscaping of parking areas</p> <p>A1 – Landscaping of parking and circulation areas must be provided where more than 5 car parking spaces are proposed. This landscaping must be no less than 5 percent of the area of the car park, except in the Central Business Zone where no landscaping is required.</p>	<p>A1 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>Clause E6.7.9 – Design of motorcycle parking areas</p> <p>A1 - The design of motorcycle parking areas must comply with all of the following:</p> <p>(a) be located, designed and constructed to comply with section 2.4.7 “Provision for Motorcycles” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;</p> <p>(b) be located within 30 m of the main entrance to the building.</p>	<p>A1 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>Clause E6.7.10 – Design of bicycle parking facilities</p> <p>A1 - The design of bicycle parking facilities must comply with all the following;</p> <p>(a) be provided in accordance with the requirements of Table E6.2;</p> <p>(b) be located within 30 m of the main entrance to the building.</p>	<p>A1 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>A2 - The design of bicycle parking spaces must be to the class specified in table 1.1 of AS2890.3-1993 Parking facilities Part 3: Bicycle parking facilities in compliance with section 2 “Design of Parking Facilities” and clauses 3.1 “Security” and 3.3 “Ease of Use” of the same Standard. R1</p>	<p>A2 – Complies.</p> <p>A condition has been recommended to ensure compliance.</p>
<p>Clause E6.7.11 – Bicycle end of trip facilities</p> <p>A1 - For all new buildings where the use requires the provision of more than 5 bicycle parking spaces</p>	<p>A1 – N/A</p> <p>The required bicycle numbers are under five (5) therefore this standard does not apply.</p>

Clause	Compliance/Comments
<p>for employees under Table E6.2, 1 shower and change room facility must be provided, plus 1 additional shower for each 10 additional employee bicycle spaces thereafter.</p>	
<p>Clause E6.7.12 – Siting of car parking A1 - Parking spaces and vehicle turning areas, including garages or covered parking areas in the Inner Residential Zone, Urban Mixed Use Zone, Village Zone, Local Business Zone and General Business Zone must be located behind the building line of buildings located or proposed on a site except if a parking area is already provided in front of the building line of a shopping centre.</p>	<p>A1 – Complies. Whilst parking is proposed in front of the building line internally, the parking is not proposed in front of the building line in terms of the building line to the frontage. It is also noted that there is already parking provided in front of the building line in the existing shopping centre.</p>
<p>Clause E6.7.13 – Facilities for commercial vehicles A1 - Commercial vehicle facilities for loading, unloading or manoeuvring must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2 : Commercial. Vehicle Facilities AS 2890.2:2002, unless: (a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site; (b) the use is not primarily dependent on outward delivery of goods from the site.</p>	<p>A1 – Complies. The carpark layout has more than one available area for designated loading bays to be provided. A condition has been recommended for the provision of at least one loading bay area to be designated.</p>
<p>Clause E6.7.14 - Access to a road A1 – Access to a road must be in accordance with the requirements of the road authority.</p>	<p>A1 – Complies.</p>
<p>E7.0 Stormwater Management Code</p>	
<p>Clause E7.7.1 - Stormwater drainage and disposal A1 – Stormwater from new impervious surfaces must be disposed of by gravity to public stormwater infrastructure.</p>	<p>A1 – Complies. A new stormwater main connection is to be provided.</p>
<p>A2 – A stormwater system for a new development must incorporate water sensitive urban design principles R1 for the treatment and disposal of stormwater if any of the following apply: (a) the size of new impervious area is more than 600 m²; (b) new car parking is provided for more than 6 cars; (c) a subdivision is for more than 5 lots.</p>	<p>A2 – Complies. Stormwater treatment system is to be provided. Refer to the Stormwater Management report.</p>
<p>A3 – A minor stormwater drainage system must be designed to comply with all of the following: (a) be able to accommodate a storm with an ARI of 20 years in the case of non-industrial zoned land and an ARI of 50 years in the case of industrial zoned land, when the land serviced by the system is fully developed; (b) stormwater runoff will be no greater than pre-existing runoff or any increase can be accommodated within existing or upgraded public stormwater infrastructure.</p>	<p>A3 – Complies. Stormwater detention system is to be provided. Refer to the Stormwater Management report.</p>

Clause	Compliance/Comments
<p>A4 – A major stormwater drainage system must be designed to accommodate a storm with an ARI of 100 years.</p>	<p>A4 – Complies. Stormwater overland flow paths for a major storm event included in the parking area design.</p>
<p>E10.0 Biodiversity Code</p>	
<p>Clause E10.7.1 - Buildings and works A1 – Clearance and conversion or disturbance must be within a Building Area on a plan of subdivision approved under this planning scheme.</p>	<p>A1 – Does not comply. The NVA (Enviro-dynamics, V4, December 2024) confirms there are moderate priority values within the footprint of the proposed development, including, potential habitat for the Chaostola skipper within the footprint of the building and one (1) tree of very high conservation value within the sewer alignment. The remaining trees of very high conservation value are located outside the Biodiversity Protection Area and only require assessment against Clause 21.4.8. The proposal is unable to meet A1 as there is no building area on the title and the proposal involves clearance and conversion of moderate priority biodiversity values, being Chaostola skipper habitat and a tree of very high conservation value. Therefore, it must be assessed against the Performance Criteria.</p>
<p>Clause E10.8.1 – Subdivision A1 - Subdivision of a lot, all or part of which is within a Biodiversity Protection Area, must comply with one or more of the following: (a) be for the purposes of separating existing dwellings; (b) be for the creation of a lot for public open space, public reserve or utility; (c) no works, other than boundary fencing works, are within the Biodiversity Protection Area; (d) the building area, bushfire hazard management area, services and vehicular access driveway are outside the Biodiversity Protection Area.</p>	<p>A1 – N/A</p>
<p>A2 - Subdivision is not prohibited by the relevant zone standards.</p>	<p>A2 – N/A</p>
<p>E15.0 Inundation Prone Areas Code</p>	
<p>Clause E15.6 - Use standards A1 – Change of use of a non-habitable building to a habitable building or a use involving habitable rooms must comply with all of the following: (a) floor level of habitable rooms is no less than the AHD level for the Coastal Inundation Low Hazard Area in Table E15.1; (b) floor level of habitable rooms is no less than the AHD level for the 1% AEP plus 300mm if in an area subject to riverine flooding.</p>	<p>A1 – N/A While part of the land is located in a Riverine Inundation Area, the proposal does not involve a change of use of a non-habitable building to a habitable building or a change of use from a non-habitable building to a use involving habitable rooms.</p>
<p>Clause E15.7.1 - Coastal inundation high hazard areas A1 – For a habitable building, including extensions to existing habitable buildings, there is no Acceptable Solution (requires assessment against performance criteria).</p>	<p>A1 – N/A The proposal does not include habitable buildings within a Coastal Inundation Hazard Area.</p>

Clause	Compliance/Comments
<p>A2 – For a non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, there is no Acceptable Solution (requires assessment against performance criteria).</p>	<p>A2 – N/A There are no non-habitable buildings or Class 10b buildings proposed within a Coastal Inundation High Hazard Area.</p>
<p>Clause E15.7.2 - Coastal inundation medium hazard areas A1 – For a new habitable building there is no Acceptable Solution (requires assessment against performance criteria).</p>	<p>A1 – N/A</p>
<p>A2 – Except for new rooms associated with habitable buildings other than dwellings, for which there is no acceptable solution, an extension to an existing habitable building must comply with one of the following: (a) new habitable rooms must comply with both of the following: (i) floor level no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1, (ii) floor area of the extension no more than 40 m² from the date of commencement of this planning scheme; (b) new habitable rooms must be above ground floor.</p>	<p>A2 – N/A</p>
<p>A3 – A non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, must have a floor area no more than 40 m².</p>	<p>A3 – N/A</p>
<p>Clause E15.7.3 - Coastal inundation low hazard areas A1 – A new habitable building must comply with the following: (a) floor level no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1;</p>	<p>A1 – N/A</p>
<p>A2 – An extension to a habitable building must comply with either of the following: (a) floor level of habitable rooms is no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1; (b) floor area is no more than 60 m².</p>	<p>A2 – N/A</p>
<p>A3 – A non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, must have a floor area no more than 60 m².</p>	<p>A3 – N/A</p>
<p>Clause E15.7.4 - Riverine inundation hazard areas A1 - A new habitable building must have a floor level no lower than the 1% AEP (100 yr ARI) storm event plus 300 mm.</p>	<p>A1 – Complies. Part of the site is located in a Riverine Inundation Area, as it is subject to subject to risk of flooding of 1% AEP or more during storm events. However, the proposed commercial building is located entirely outside this area. Therefore, this clause is not applicable. Notwithstanding, the proposed commercial building has a floor level 300mm above the 1% storm event</p>

Clause	Compliance/Comments
	level and stormwater overland flow paths for 1% major storm event included in the parking area design.
<p>A2 – An extension to an existing habitable building must comply with one of the following:</p> <p>(a) floor level of habitable rooms is no lower than the 1% AEP (100 yr ARI) storm event plus 300 mm;</p> <p>(b) floor area of the extension no more than 60 m² as at the date of commencement of this planning scheme.</p>	<p>A2 – N/A</p> <p>The proposal does not include an extension to a habitable building within a Riverine Inundation Hazard Area.</p>
<p>A3 – The total floor area of all non-habitable buildings, outbuildings and Class 10b buildings under the Building Code of Australia, on a site must be no more than 60 m².</p>	<p>A3 – N/A</p> <p>There are no non-habitable buildings, outbuildings or class 10b structures within a Riverine Inundation Hazard Area as part of the proposal.</p>
<p>Clause E15.7.5 - Riverine, coastal investigation area, low, medium high inundation hazard area</p> <p>A1 – For landfill, or solid walls greater than 5 m in length and 0.5 m in height, there is no acceptable solution (requires assessment against performance criteria).</p>	<p>A1 – Complies.</p> <p>There is no landfill or solid walls greater than 5m length and 0.5m high in the area affected by the code.</p>
<p>A2 – No acceptable solution (requires assessment against performance criteria)</p>	<p>A2 – N/A</p> <p>The proposal does not involve or rely upon any mitigation measures.</p>
<p>A3 – A land application area for onsite wastewater management must comply with all of the following:</p> <p>(a) horizontal separation distance from high water mark or from the top of bank of a watercourse or lake must be no less than 100 m;</p> <p>(b) vertical separation distance from the water table must be no less than 1.5 m.</p>	<p>A3 – N/A</p> <p>Site is connected to services.</p>
<p>Clause E15.7.6 - Development Dependent on a Coastal Location</p> <p>A1 - An extension to an existing boat ramp, car park, jetty, marina, marine farming shore facility or slipway must be no more than 20% of the size of the facility existing at the effective date.</p>	<p>A1 – N/A</p> <p>The proposal does not include development dependent on a coastal location.</p>
<p>A2 - No acceptable solution (requires assessment against performance criteria).</p>	<p>A2 – N/A</p> <p>The proposal does not involve any dredging or reclamation.</p>
<p>A3 - No Acceptable Solution for coastal protection works initiated by the private sector (requires assessment against performance criteria).</p>	<p>A3 – N/A</p> <p>The proposal does not involve any coastal protection works.</p>
<p>E17.0 Signs Code</p>	
<p>Clause E17.6.1 – Use of Signs</p> <p>A1 – A sign must be a permitted sign in Table E.17.3.</p>	<p>A1 – Complies.</p> <p>Wall signs and above awning signs are permitted in the General Business Zone in Table E.17.3.</p>
<p>A2 - A sign associated with the sale of goods or services must relate directly to the use of the building or site to which it is affixed.</p>	<p>A2 – Complies.</p> <p>Signs would be business identification signs</p>
<p>A3 - A sign must not contain flashing lights, moving parts or moving or changing messages or graphics, except if a Statutory Sign</p>	<p>A3 – Complies.</p> <p>No flashing lights, moving parts or moving or changing messages or graphics proposed.</p>

Clause	Compliance/Comments
<p>A4 - An illuminated sign must not be located within 30 metres of a residential use, except if a Statutory Sign</p>	<p>A4 – N/A Signs are not proposed to be illuminated.</p>
<p>Clause E17.7.1 - Standards for Signs A1 - A sign must comply with the standards listed in Table E.17.2 and be a permitted sign in Table E17.3.</p>	<p>A1 – Does not comply. The signs would exceed the sizes listed in Table E.17.2.</p>
<p>A2 - The number of signs per business per street frontage must comply with all of the following:</p> <ul style="list-style-type: none"> (a) maximum of 1 of each sign type; (b) maximum of 1 window sign per window; (c) if the street frontage is less than 20 m in length, the maximum number of signs on that frontage is 3; (d) if the street frontage is 20 m in length or greater, the maximum number of signs on that frontage is 6. <p>except for the following sign types, for which there is no limit;</p> <ul style="list-style-type: none"> (i) Building Site, (ii) Name Plate, (iii) Newspaper Day Bill, (iv) Open/Closed, (v) Real Estate, (vi) Street Number, (vii) Temporary Sign. 	<p>A2 – Complies. Two wall signs are proposed for two separate tenancies (Showrooms 1 and 2) on the Maranoa Road.</p>
<p>A3 - Signs must not obscure or prevent or delay a driver from seeing a Statutory Sign or a Tourist Information Sign.</p>	<p>A3 – Complies. Signs are wall signs integrated into the buildings.</p>
<p>A4 - Signs must not resemble Statutory Signs because of the same or similar shape, size, design, colour, letter size or lighting.</p>	<p>A4 – Complies. Signs are wall signs integrated into the buildings.</p>
<p>Clause E17.7.2 - Standards for signs on Heritage Places subject to the Heritage Code or within Heritage Precincts or Cultural Landscape Precincts A1 - No acceptable solution (requires assessment against performance criteria)</p>	<p>A1 – N/A The site is not heritage listed.</p>

Note: Codes not listed in this Checklist have been assessed as not being relevant to the assessment of this application.

DRAWING NO:	DESCRIPTION
A1.0	Location Plan
A2.0	Site Plan
A3.0	Warehouses 1&2 & Showroom 01 Ground Floor Plan
A4.0	Warehouse 1&2 & Showroom 01 First Floor Plan
A5.0	Warehouses 1&2 & Showroom 01 Roof Plan
A6.0	Warehouses 3-8 Ground Floor Plan
A7.0	Warehouses 3-8 First Floor Plan
A8.0	Warehouses 3-8 Roof Plan
A9.0	Warehouses & Showroom 01 Elevations
A10.0	Warehouses & Showroom 01 Elevations
A11.0	Warehouse 8 & Showroom 01 Elevations
A12.0	Warehouses & Showroom 01 - Glazing Calculation
A13.0	Showroom 02 Floor Plan
A14.0	Showroom 02 Roof Plan
A15.0	Showroom 02 Elevations
A16.0	Showroom 02 - Glazing Calculation
A17.0	3D Visualisations
A18.0	3D Visualisations
A19.0	RFI Response - Streetscape



Artist Impression Only



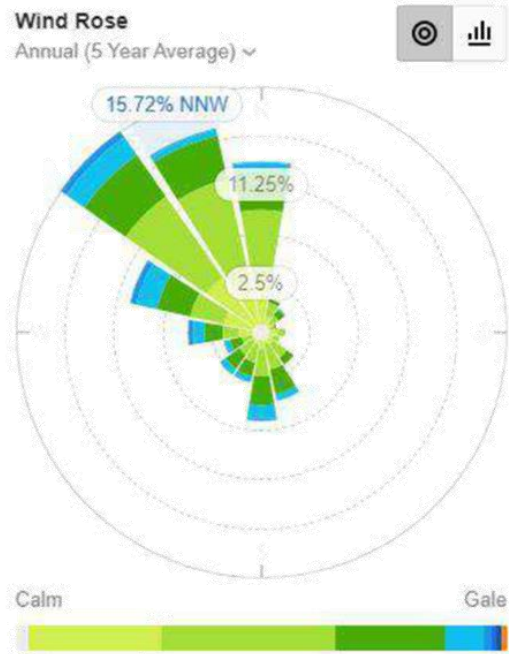
**37-59 MARANOA ROAD, KINGSTON
PROPOSED WAREHOUSES + SHOWROOMS**

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

SITE INFORMATION

Title Reference: 126707/0
 Planning Zone: General Business Zone
 General Overlay: N/A
 Code Overlay:
 - Biodiversity Protection Area
 Wind Classification: TBC
 Soil Classification: Refer to XXX Report for further informations
 Climate Zone: 7
 BAL Level: N/A
 Site Area: TBC m²
 Total Building Area: 2526.05m²

WIND ROSE



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

Licence: 189009392



CLIENT: REVELSTOKE BUILDING SOLUTIONS
 ADDRESS: 37-59 MARANOA ROAD, KINGSTON

JOB NO: xxx
 PROPOSAL: NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE: DA

DATE: 27/09/2024
 SCALE:

REVISION: C



A1.0

Location Plan

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

LEGEND

- Proposed warehouse
- Planning setback
- Proposed driveway
- Landscaping
- Boundary
- Existing Cable Hydro Underground
- Existing Cable Telstra Underground
- Proposed Right of Way
- DBH - Tree to be retained
- DBH - Tree to be removed
- Tree Protection Zone - Tree to be retained
- Tree Protection Zone - Tree to be removed

NOTE:
PLEASE REFER TO NATURAL VALUES ASSESSMENT PREPARED BY ENVIRO-DYNAMICS FOR FURTHER DETAILS.

NOTE:
PLEASE REFER TO CIVIL DRAWINGS AND STORMWATER MANAGEMENT PLAN (SWMP) PREPARED BY FLUSSIG FOR FURTHER CIVIL ENGINEERING DETAILS.

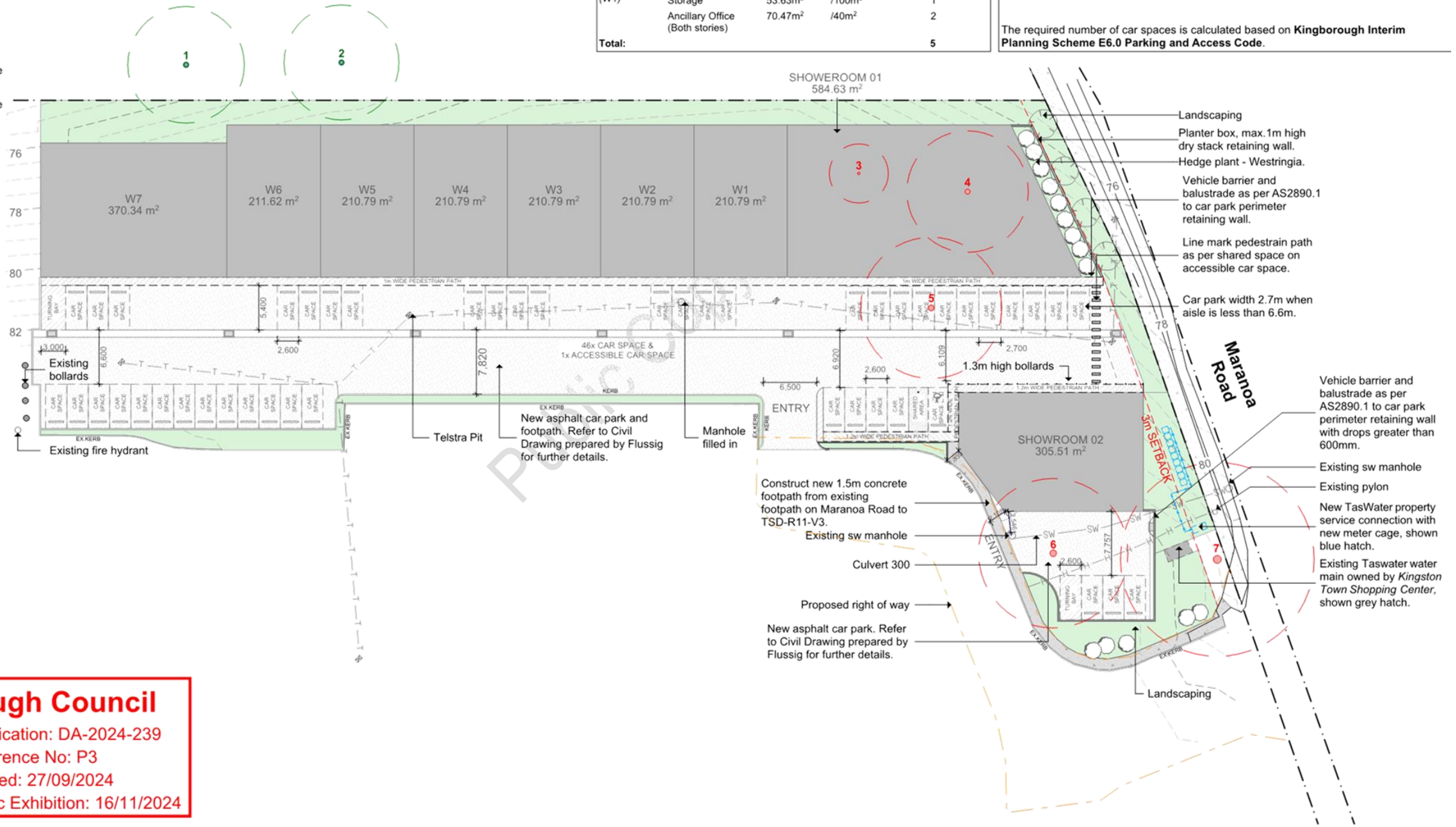
NUMBER OF CAR PARKING SPACE			
USE CLASS:	AREA:	RATE	NUMBER OF CAR PARKING SPACE
Showroom 1	Bulky Goods Sales	376.65m ² /50m ²	8
	Storage (Incl.Mezzanine)	204.71m ² /100m ²	2
	Ancillary Office	34.51m ² /40m ²	1
Total:			11
Showroom 2	Bulky Goods Sales	147.18m ² /50m ²	3
	Storage (Incl.Mezzanine)	155.82m ² /100m ²	2
	Ancillary Office	31.88m ² /40m ²	1
Total:			6
Warehouse 1 (W1)	Bulky Goods Sales	112.78m ² /50m ²	2
	Storage	53.63m ² /100m ²	1
	Ancillary Office (Both stories)	70.47m ² /40m ²	2
Total:			5

NUMBER OF CAR PARKING SPACE			
USE CLASS:	AREA:	RATE	NUMBER OF CAR PARKING SPACE
Warehouses Storage 2-6 (W2 - W6)	786.65m ² (157.33m ² per warehouse)	/100m ²	8
	Ancillary Office (Both stories)	352.35m ² (70.47m ² per warehouse)	/40m ²
Total:			17
Warehouse 7 (W7)	Storage (Incl.Mezzanine)	357.64m ² /100m ²	4
	Ancillary Office	39.1m ² per warehouse /40m ²	1
Total:			5
Total Required Car Space:			44
Proposed Car Space:			46

The required number of car spaces is calculated based on Kingborough Interim Planning Scheme E6.0 Parking and Access Code.

EXISTING TREE SCHEDULE

NO.	SPECIES
1	Acacia melanoxylon
2	stag (dead tree, species unknow)
3	Eucalyptus amygdalina
4	Eucalyptus tenuiramis
5	Eucalyptus amygdalina
6	Eucalyptus tenuiramis
7	Eucalyptus amygdalina



Kingborough Council
Development Application: DA-2024-239
Plan Reference No: P3
Date Received: 27/09/2024
Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
DRAFTING & DESIGN
P: 0472 655 173
E: admin@matt-kennedy.com.au
Licence: 18900392

bdaa
BUILDING DESIGNERS
ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
JOB NO: **xxx**
ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
PROJECT STAGE: **DA**

DATE: **27/09/2024**
SCALE: **1:500@A3**

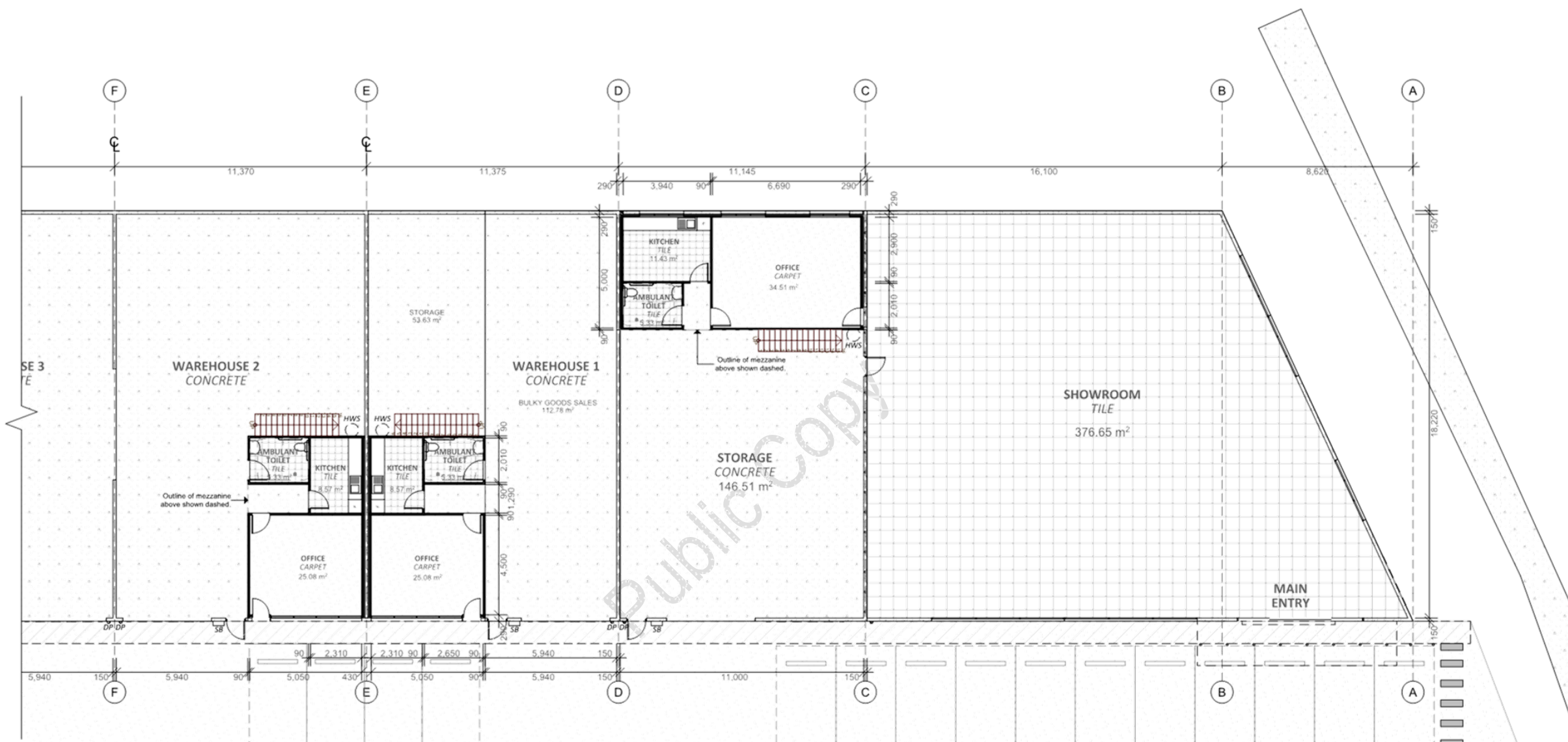
REVISION: **C**



A2.0
Site Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council

Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 13900392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

DATE: **27/09/2024**
 SCALE: **1:200@A3**

REVISION: **C**

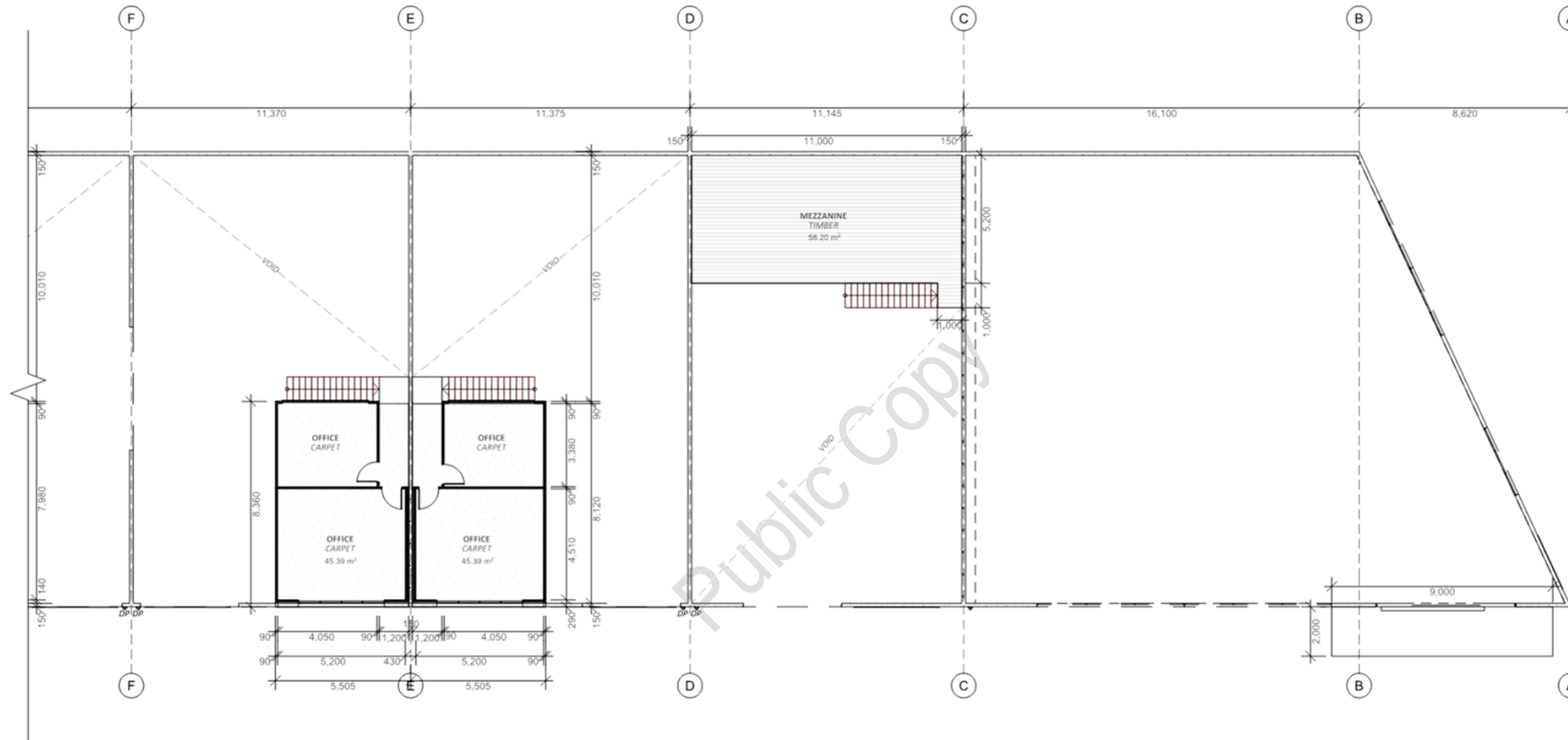


A3.0

Warehouses 1&2 & Showroom 01 Ground Floor Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

DATE: **27/09/2024**
 SCALE: **1:200@A3**

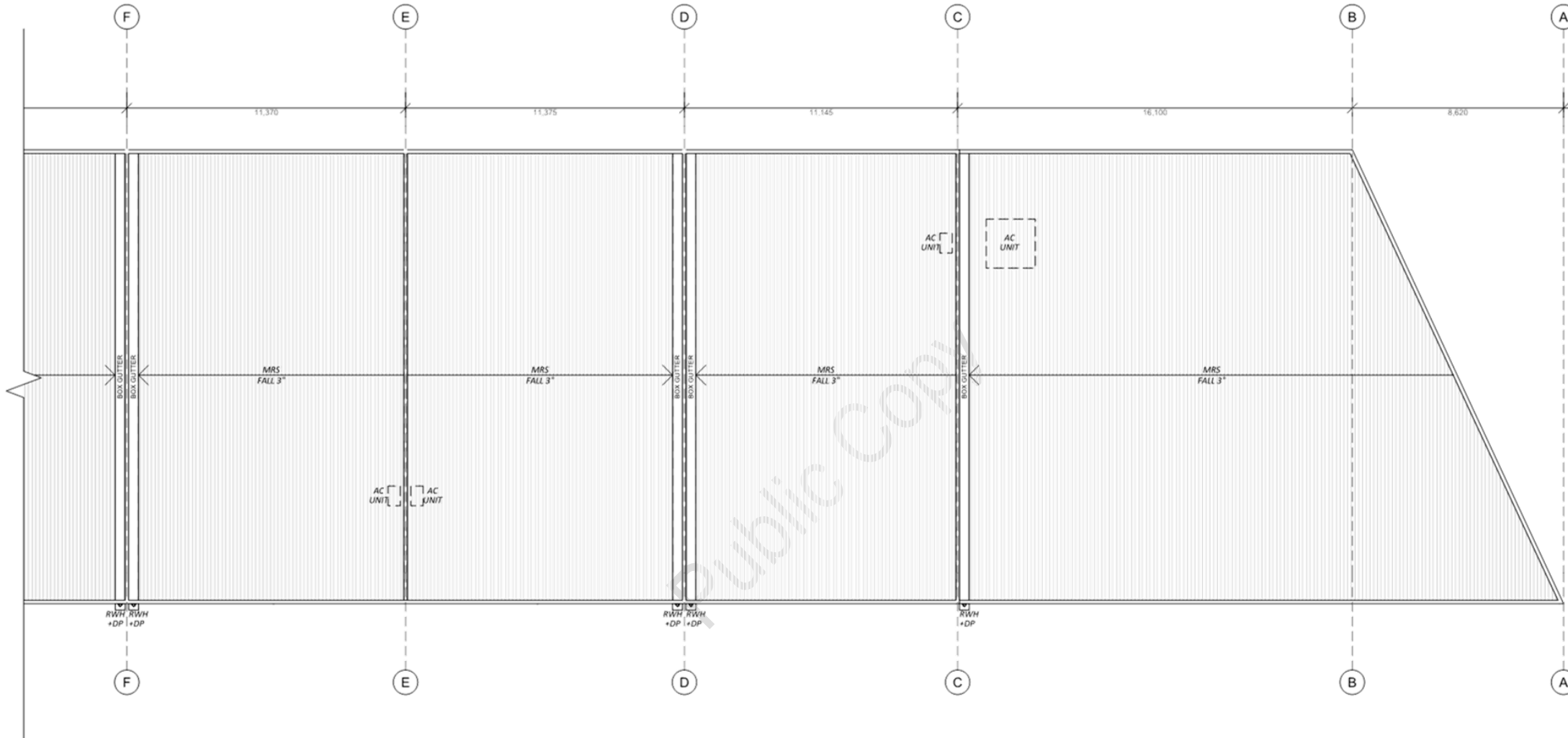
REVISION: **C**



A4.0
 Warehouse 1&2 &
 Showroom 01 First Floor
 Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE **DA**

DATE **27/09/2024**
 SCALE **1:200@A3**

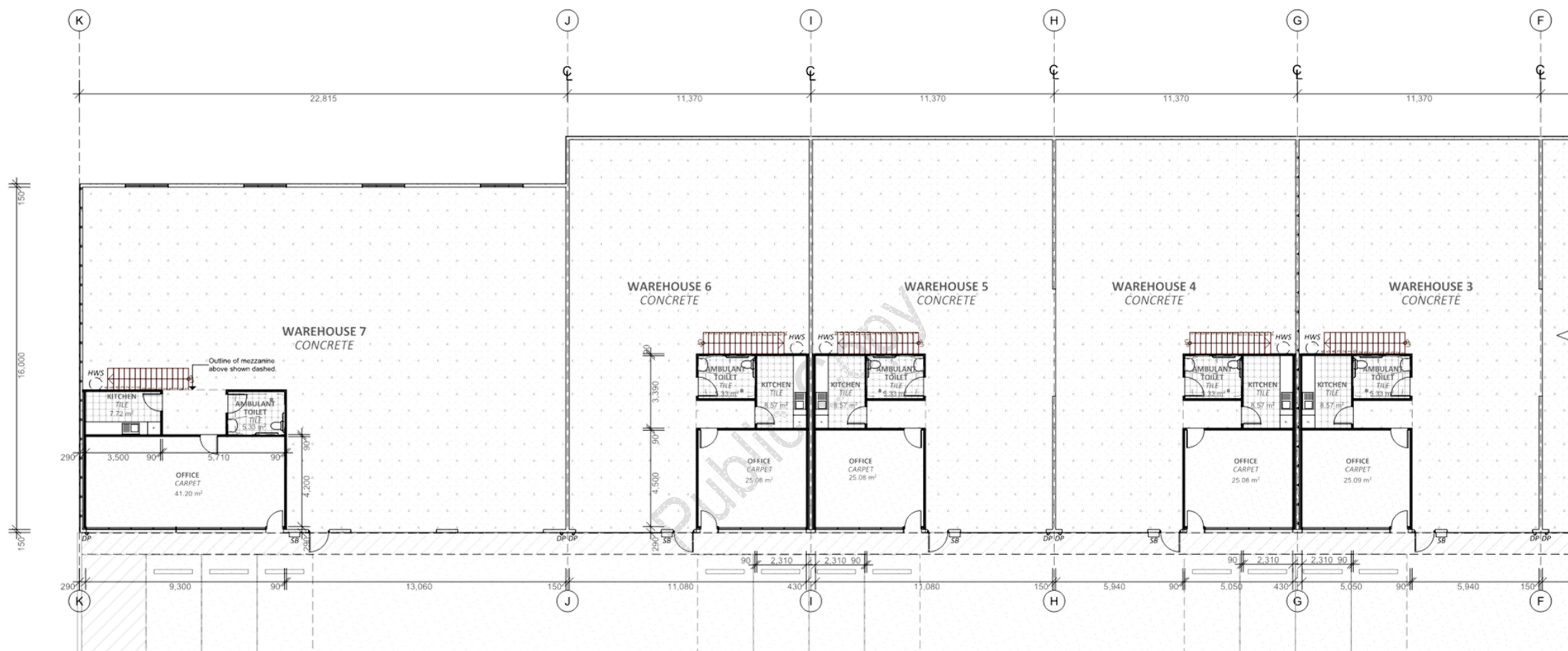
REVISION **C**



A5.0
 Warehouses 1&2 & Showroom 01 Roof Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheetting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council

Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 139009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

DATE: **27/09/2024**
 SCALE: **1:200@A3**

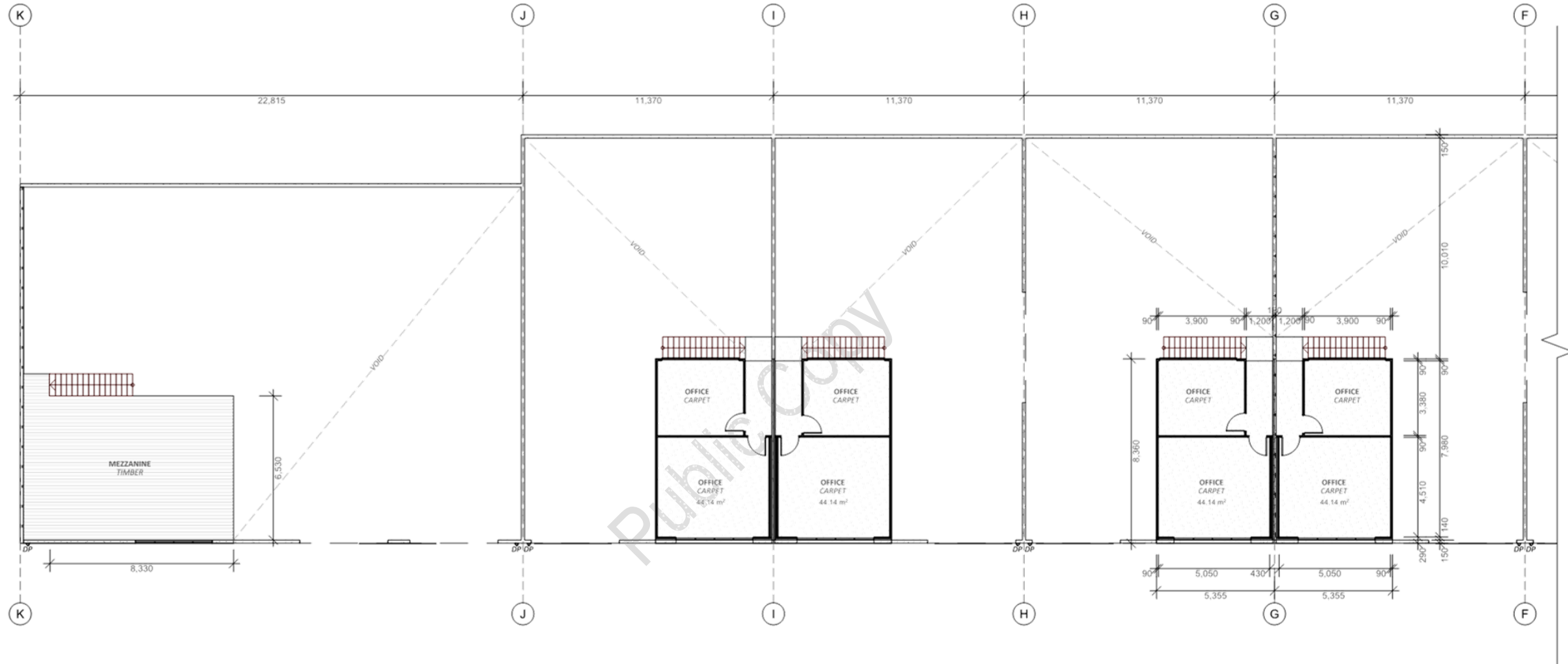
REVISION: **C**



A6.0

Warehouses 3-8 Ground Floor Plan

- KEY**
- DP Rainwater Downpipe
 - FFL Finished Floor Level
 - HWS Hot Water System
 - MRS Metal Roof Sheeting
 - RWH Rainwater Head
 - SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

DATE: **27/09/2024**
 SCALE: **1:200@A3**

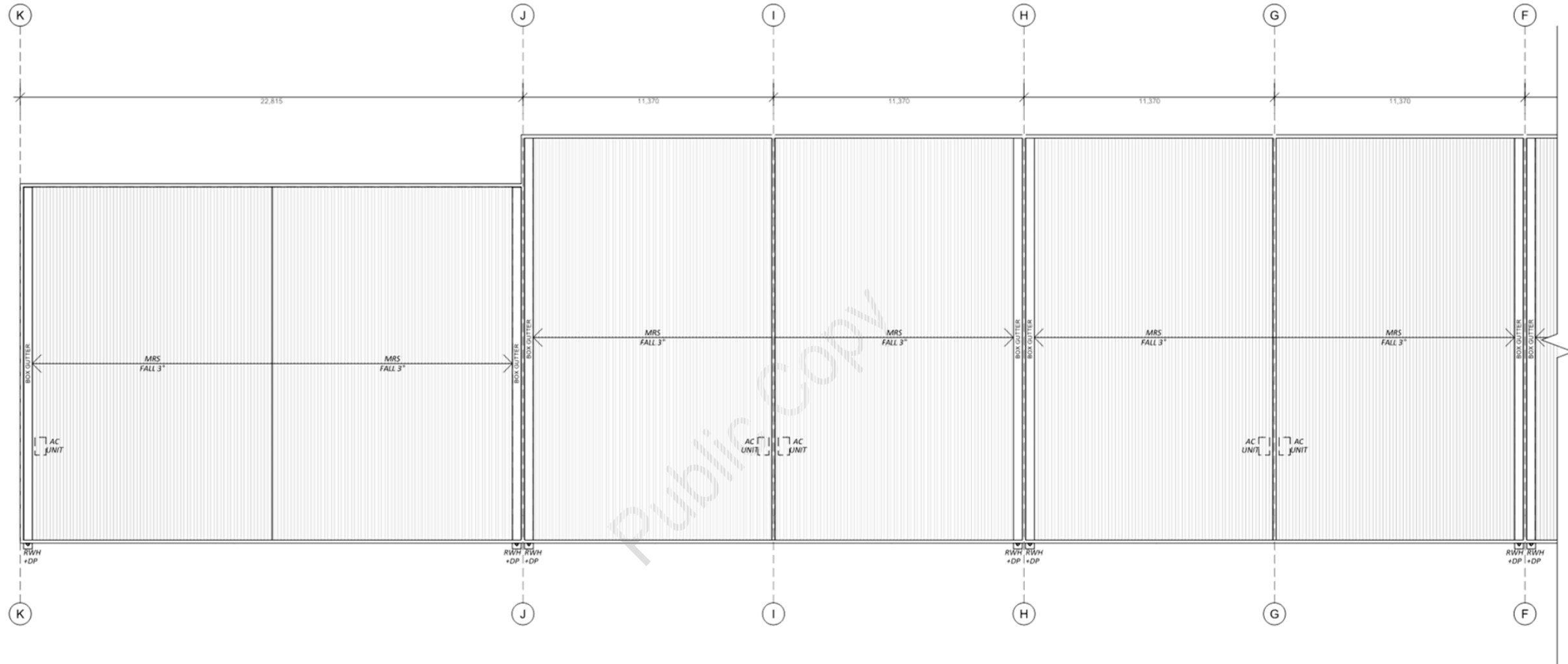
REVISION: **C**



A7.0
 Warehouses 3-8 First Floor
 Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE **DA**

DATE **27/09/2024**
 SCALE **1:200@A3**

REVISION **C**

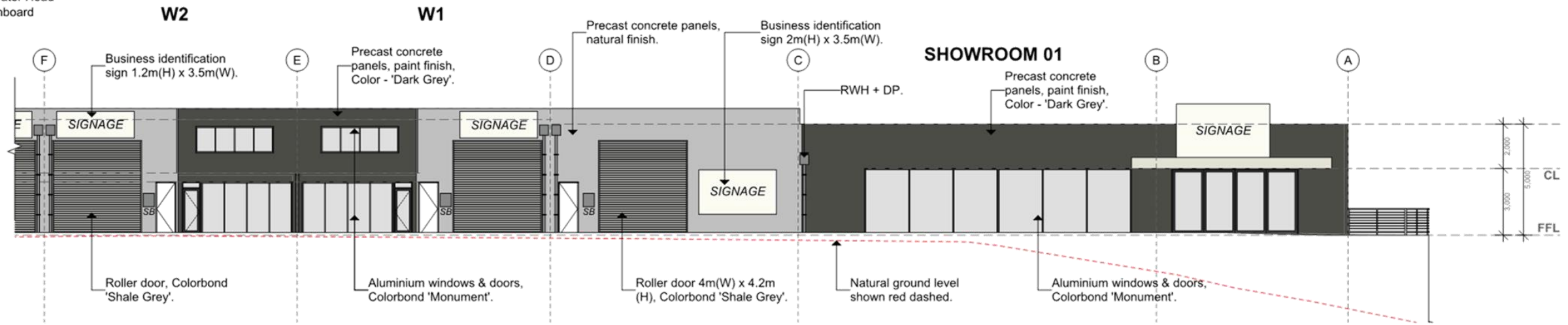


A8.0

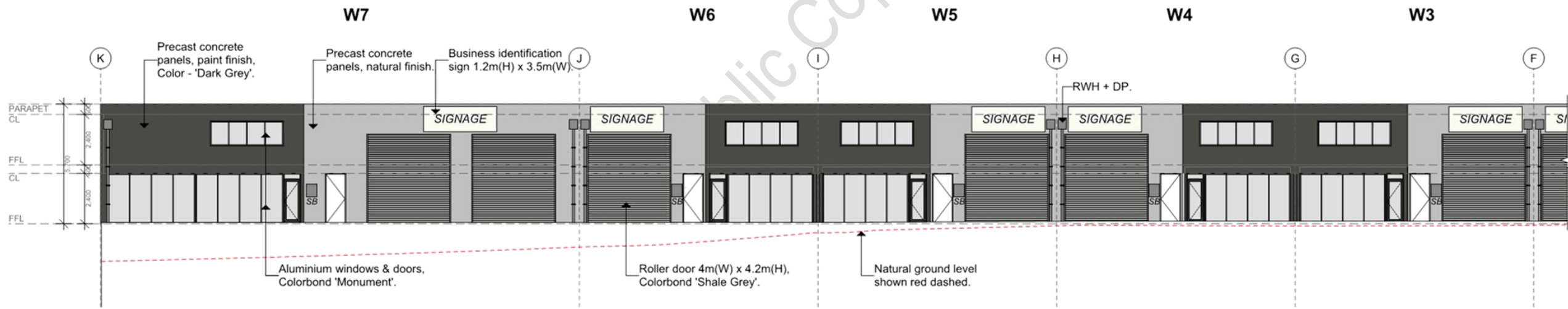
Warehouses 3-8 Roof Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



1
-
SW Elevation
1:200 @ A3



2
-
SW Elevation
1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT:
REVELSTOKE BUILDING SOLUTIONS
 ADDRESS:
37-59 MARANOA ROAD, KINGSTON

JOB NO:
xxx
 PROPOSAL
NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE
DA

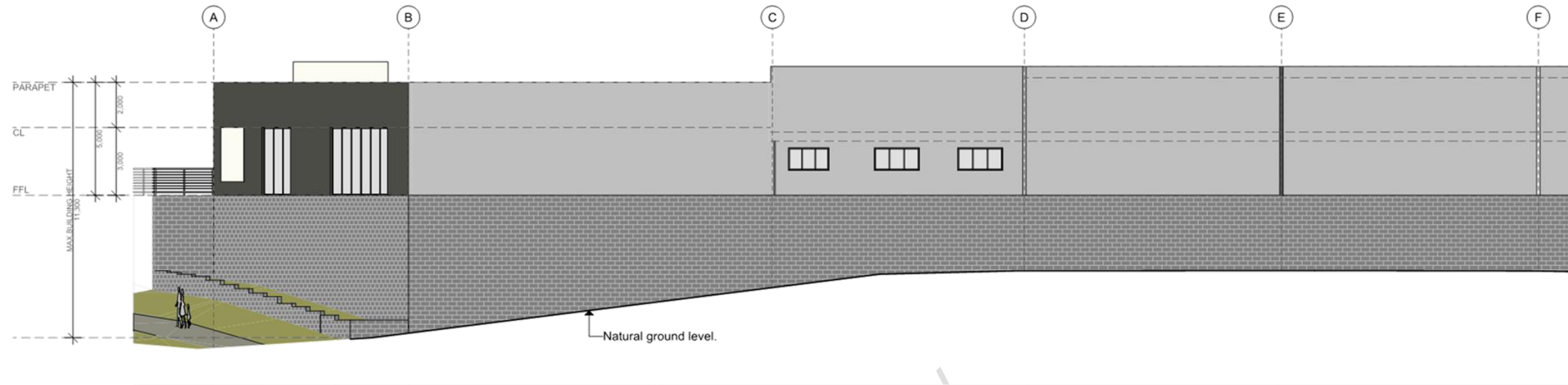
DATE
27/09/2024
 SCALE
1:200@A3

REVISION
C

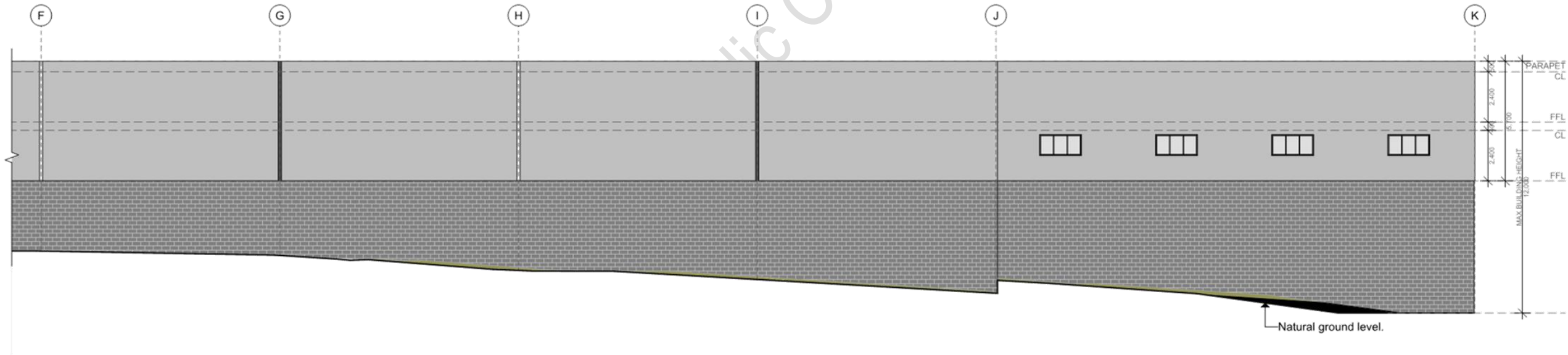
A9.0
 Warehouses & Showroom
 01 Elevations

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



1
-
NE Elevation
1:200 @ A3



2
-
NE Elevation
1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



CLIENT:
REVELSTOKE BUILDING SOLUTIONS
 ADDRESS:
37-59 MARANOA ROAD, KINGSTON

JOB NO:
xxx
 PROPOSAL
NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE
DA

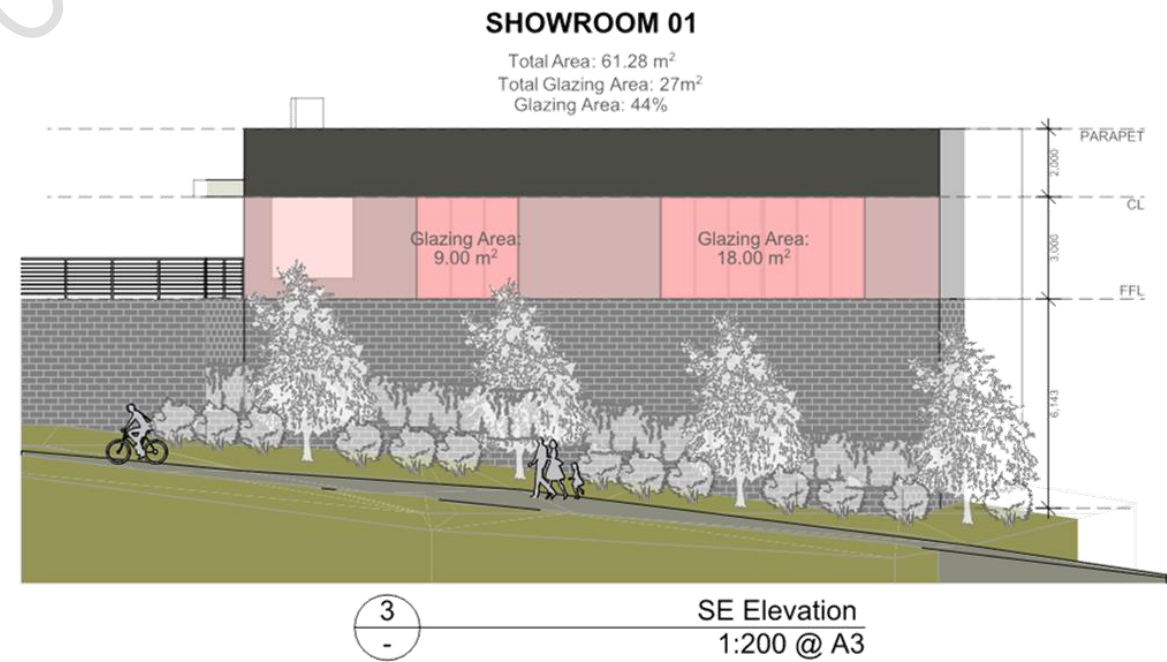
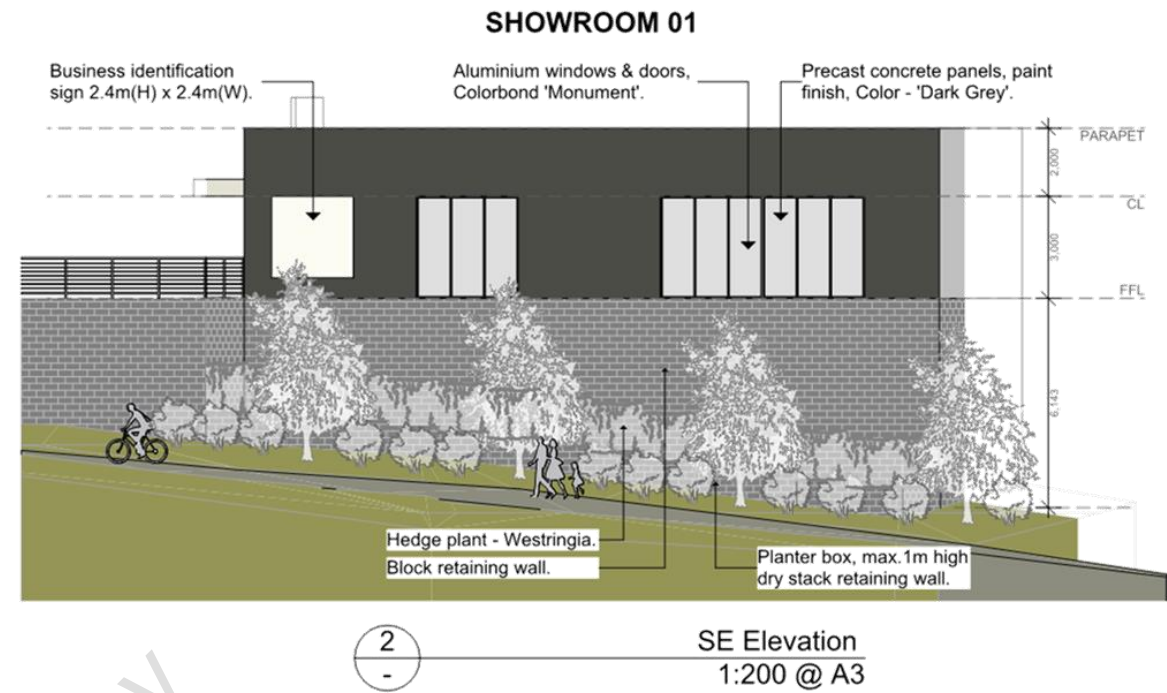
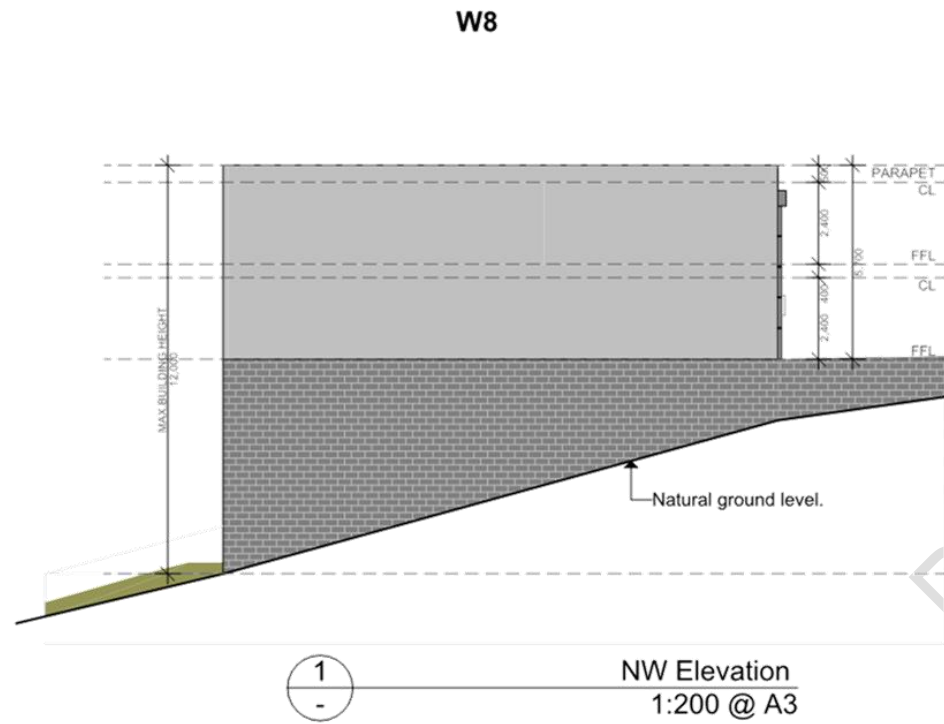
DATE
27/09/2024
 SCALE
1:200@A3

REVISION
C

A10.0

Warehouses & Showroom
01 Elevations

- KEY**
- DP Rainwater Downpipe
 - FFL Finished Floor Level
 - HWS Hot Water System
 - MRS Metal Roof Sheeting
 - RWH Rainwater Head
 - SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024



CLIENT: REVELSTOKE BUILDING SOLUTIONS
 ADDRESS: 37-59 MARANOVA ROAD, KINGSTON

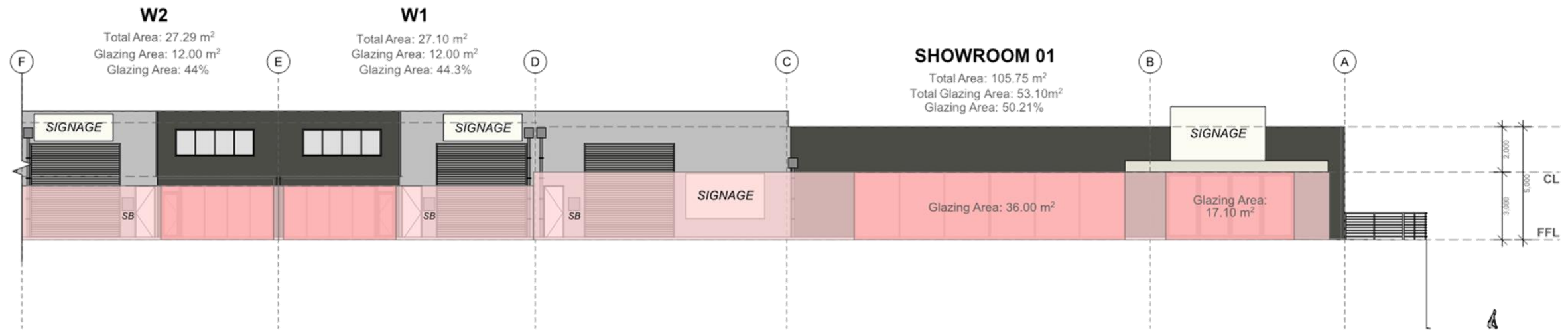
JOB NO: xxx
 PROPOSAL: NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE: DA

DATE: 27/09/2024
 SCALE: 1:200@A3

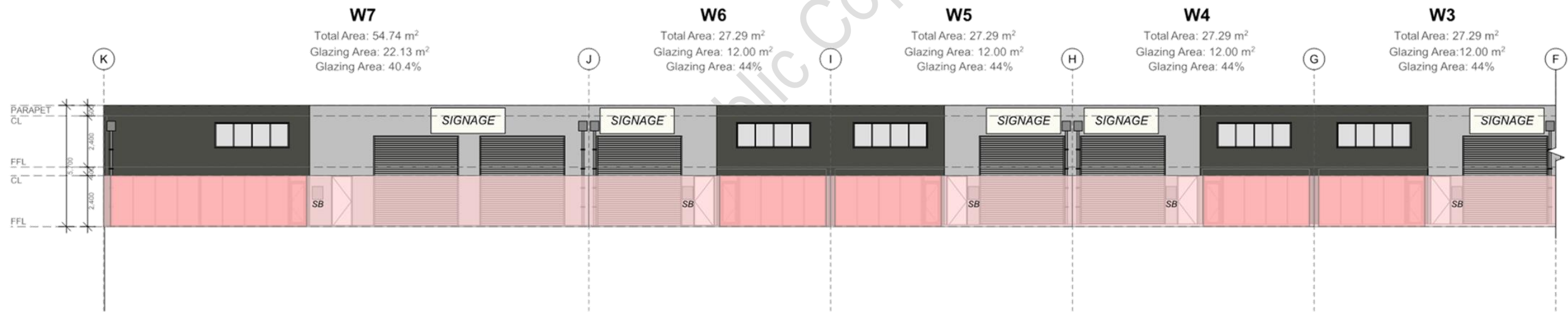
REVISION: C

A11.0
 Warehouse 8 & Showroom
 01 Elevations

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



1 - South Elevation
1:200 @ A3



2 - South Elevation
1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

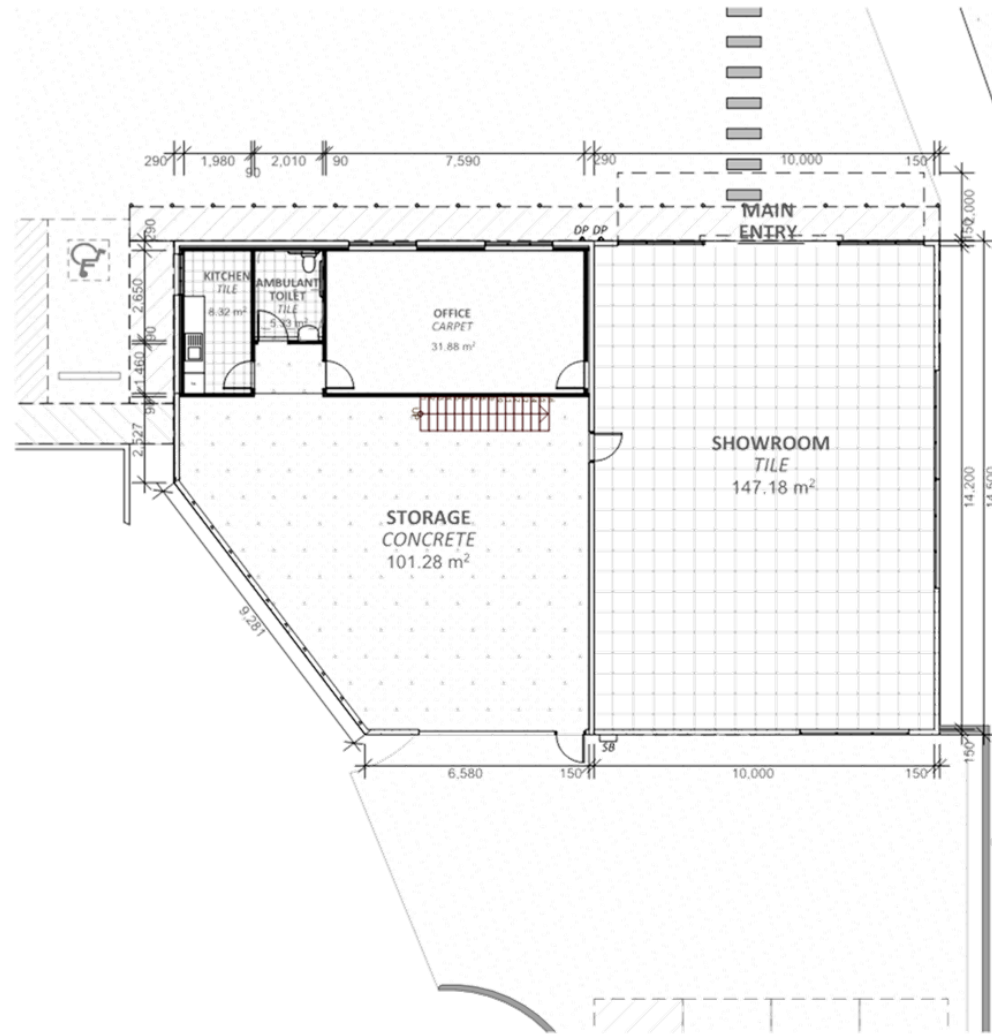
DATE: **27/09/2024**
 SCALE: **1:200@A3**

REVISION: **C**

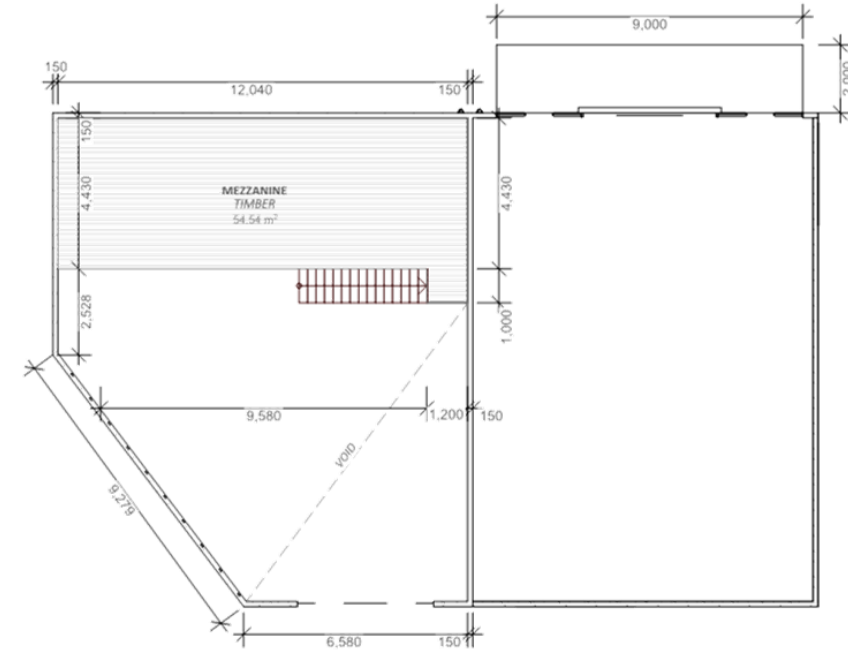
A12.0
 Warehouses & Showroom
 01 - Glazing Calculation

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



1 - Showroom 02 - Ground Floor
1:200 @ A3



2 - Showroom 02 - First Floor
1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 139009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 JOB NO: **xxx**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

PROPOSAL **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE **DA**

DATE **27/09/2024**
 SCALE **1:200@A3**

REVISION **C**

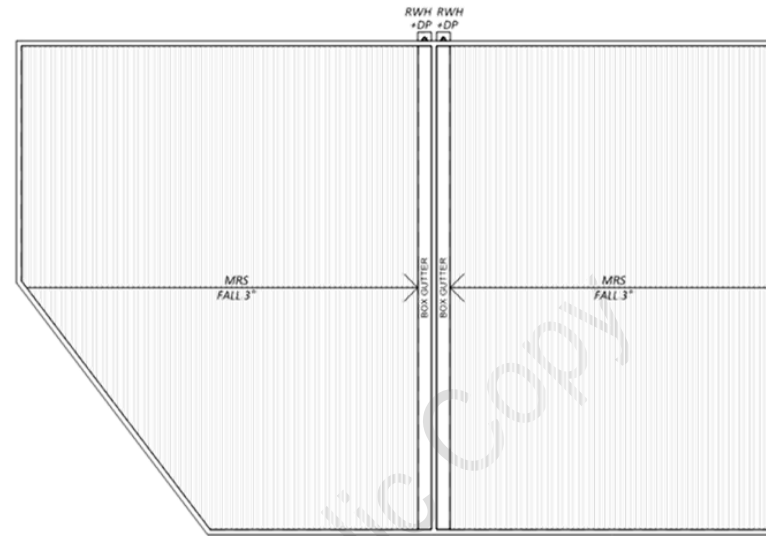


A13.0

Showroom 02 Floor Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



2 — Showroom 02 - First Floor
- 1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOVA ROAD, KINGSTON**

JOB NO: **xxx**
 PROPOSAL: **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE: **DA**

DATE: **27/09/2024**
 SCALE: **1:200@A3**

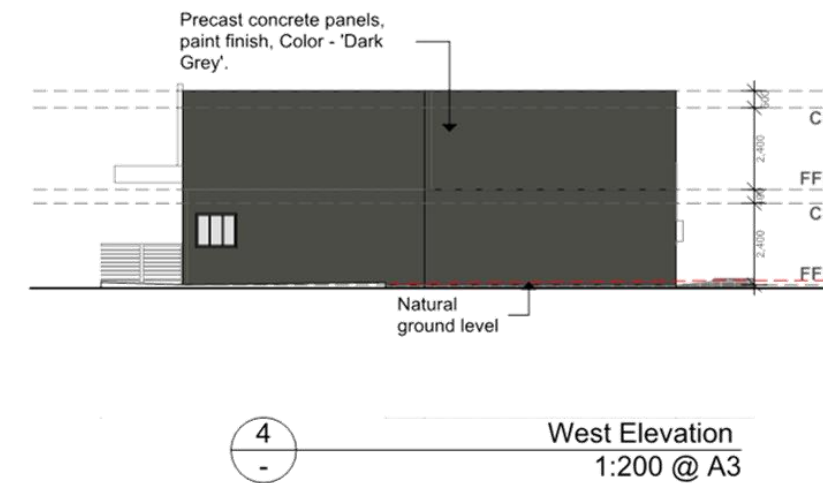
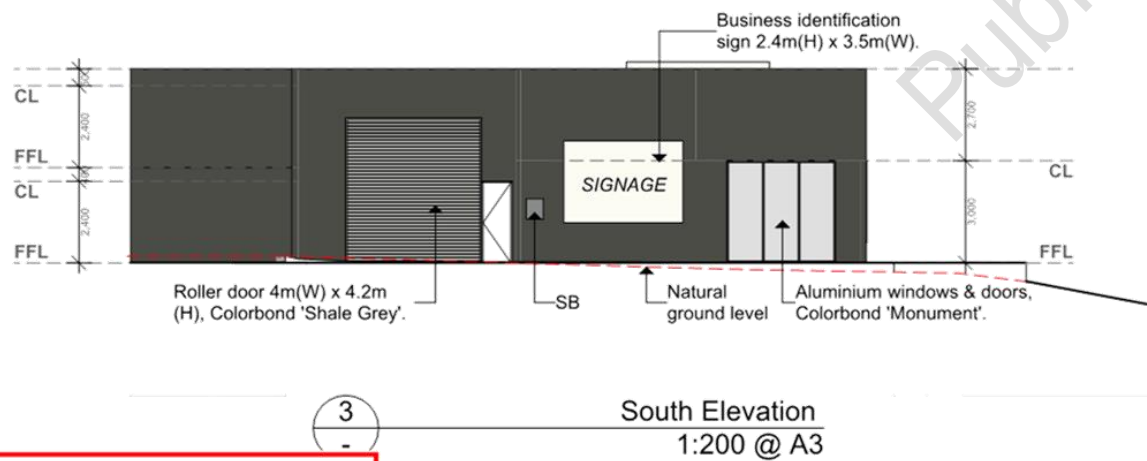
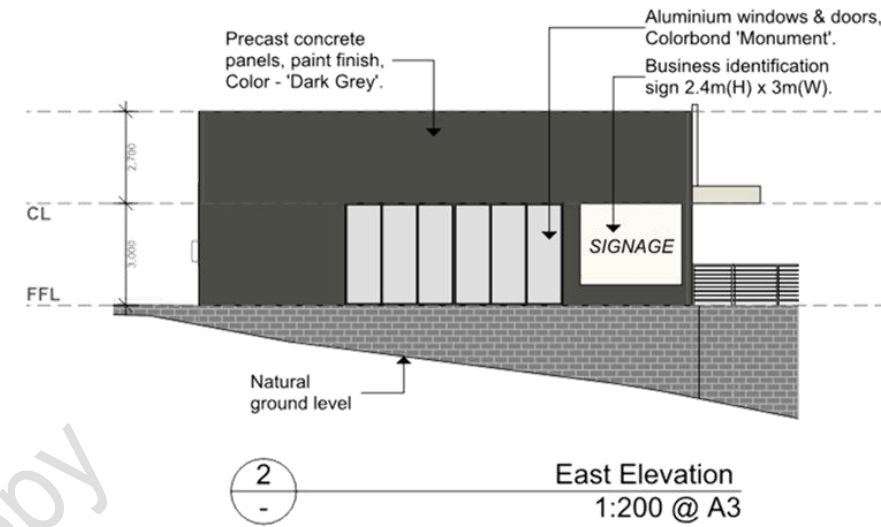
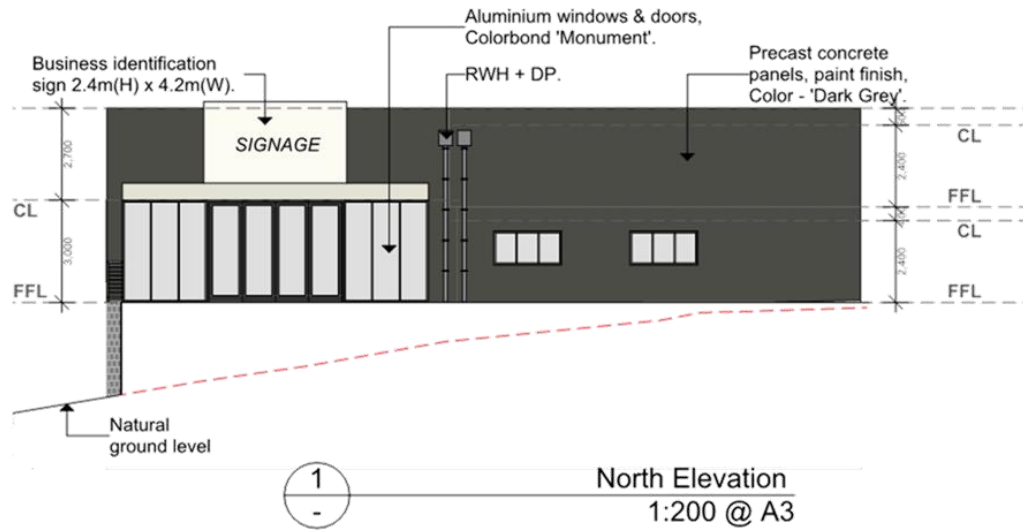
REVISION: **C**



A14.0
 Showroom 02 Roof Plan

KEY

- DP Rainwater Downpipe
- FFL Finished Floor Level
- HWS Hot Water System
- MRS Metal Roof Sheeting
- RWH Rainwater Head
- SB Switchboard



Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024

MK | **MATT KENNEDY**
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392

bdaa
 BUILDING DESIGNERS
 ASSOCIATION OF AUSTRALIA

CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 ADDRESS: **37-59 MARANOVA ROAD, KINGSTON**

JOB NO: **xxx**

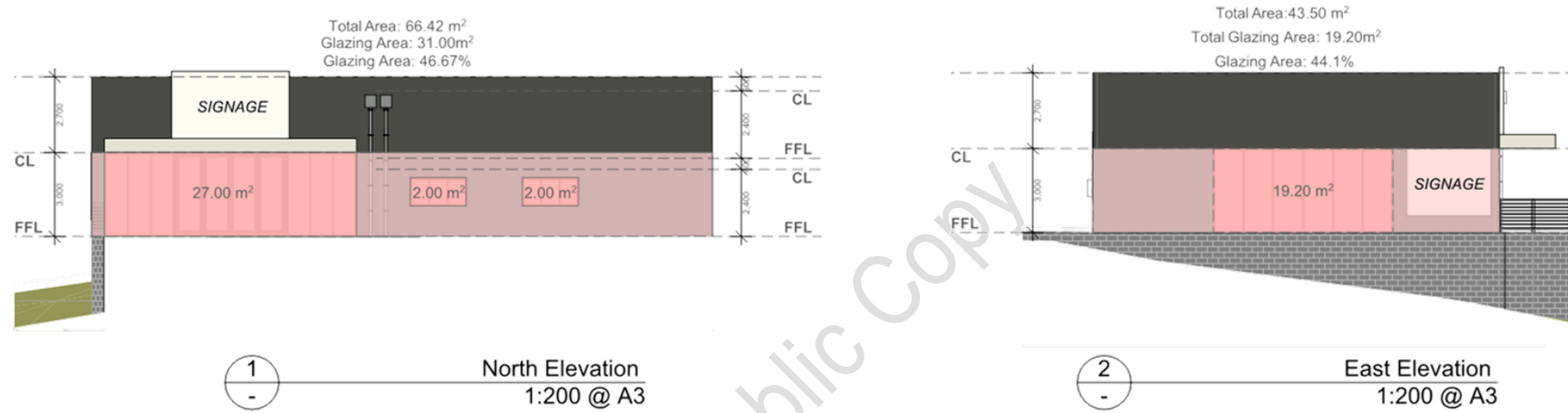
PROPOSAL **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE **DA**

DATE **27/09/2024**
 SCALE **1:200@A3**

REVISION **C**

A15.0

Showroom 02 Elevations



Public Copy

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



MATT KENNEDY
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392



CLIENT:
REVELSTOKE BUILDING SOLUTIONS
 ADDRESS:
37-59 MARANOA ROAD, KINGSTON

JOB NO:
xxx

PROPOSAL
NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE
DA

DATE
27/09/2024
 SCALE
1:200@A3

REVISION
C

A16.0

Showroom 02 - Glazing
 Calculation



Artist Impression

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



MATT KENNEDY
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 18900392



CLIENT:
REVELSTOKE BUILDING SOLUTIONS
 ADDRESS:
37-59 MARANOVA ROAD, KINGSTON

JOB NO:
xxx

PROPOSAL
NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE
DA

DATE
27/09/2024
 SCALE

REVISION
C

A17.0

3D Visualisations



Artist Impression

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



MATT KENNEDY
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392



CLIENT: **REVELSTOKE BUILDING SOLUTIONS**
 JOB NO: **xxx**
 ADDRESS: **37-59 MARANOA ROAD, KINGSTON**

PROPOSAL **NEW WAREHOUSES & SHOWROOMS**
 PROJECT STAGE **DA**

DATE **27/09/2024**
 SCALE

REVISION **C**

A18.0

3D Visualisations



3D VISUALISATION

Artist Impression



STREETSCAPE - MARANOA ROAD
1:200 @ A3

Kingborough Council
 Development Application: DA-2024-239
 Plan Reference No: P3
 Date Received: 27/09/2024
 Date placed on Public Exhibition: 16/11/2024

This drawing is the property of Matt Kennedy Drafting & Design, reproduction in whole or part is strictly forbidden without written consent. © 2024



MATT KENNEDY
 DRAFTING & DESIGN
 P: 0472 655 173
 E: admin@matt-kennedy.com.au
 Licence: 189009392



CLIENT:
REVELSTOKE BUILDING SOLUTIONS
 ADDRESS:
37-59 MARANOA ROAD, KINGSTON

JOB NO:
xxx
 PROPOSAL
NEW WAREHOUSES & SHOWROOMS
 PROJECT STAGE
DA

DATE
27/09/2024
 SCALE
1:200@A3

REVISION
C

A19.0

RFI Response - Streetscape



RESULT OF SEARCH

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SEARCH OF TORRENS TITLE

VOLUME 126707	FOLIO 0
EDITION 7	DATE OF ISSUE 03-Feb-2022

SEARCH DATE : 27-Jun-2024

SEARCH TIME : 09.31 AM

DESCRIPTION OF LAND

Town of KINGSTON
 The Common Property for Strata Scheme 126707
 Derivation : Part of 212 Acres Gtd to J. Lucas and Part of 234
 Acres Gtd to W.T. Firth
 Prior CT 27282/10

SCHEDULE 1

STRATA CORPORATION NUMBER 126707, KINGSTON TOWN SHOPPING
 CENTRE, KINGSTON

SCHEDULE 2

Reservations and conditions in the Crown Grant if any
 The proprietor holds the common property subject to any
 interests for the time being notified on the said
 Stratum Plan and subject to any amendments to common
 property shown on that Plan

SP182287 EASEMENTS in Schedule of Easements
 SP182287 COVENANTS in Schedule of Easements
 SP182287 FENCING PROVISION in Schedule of Easements
 E160163 BENEFITING EASEMENT: a Water Supply Easement
 (appurtenant to the land marked ABCDE on Sealed Plan
 177320) over the land marked Private Service Easement
 to Supply Water 2.00 wide on Plan 181418

E160163 BENEFITING EASEMENT: a Services Easement (appurtenant
 to the land marked ABCDE on Sealed Plan 177320) over
 the land marked Private Service Easement 1.00 wide on
 Plan 181418 Registered 13-Aug-2021 at noon

SP 7485 FENCING PROVISION in Schedule of Easements
 M770691 APPLICATION by body corporate to amend strata plan
 126707 by excising Lot 1 on Sealed Plan 177320 from
 the common property Registered 22-Jul-2019 at noon

E183056 LEASE to EG FUELCO (AUSTRALIA) LIMITED of a leasehold
 estate for the term of 10 years from 08-Sep-2018 (of
 that part of the said land within shown hatched on
 the plan attached to the said lease) (Together with a
 right of carriageway) Registered 16-Mar-2020 at noon

**RESULT OF SEARCH**

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980

E277703 APPLICATION by body corporate to amend Strata Plan by excising Lot 1 on Sealed Plan 182287 from the common property Registered 03-Feb-2022 at noon

C109215 The Recorder of Titles has not transferred the endorsements (if any) shown on the Strata Plan to this folio. Search the Strata Plan.

UNREGISTERED DEALINGS AND NOTATIONS

Lodged by LTO on 26-Jun-2006 BP: \$9692122

181418 PLAN Lodged by BUTLER MCINTYRE & B on 15-Jun-2021
BP: 181418

Public Copy



FOLIO PLAN

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



24 DEC 2021

CITY/TOWN KINGSTON SUBURB KINGSTON FOLIO REFERENCE FR 27202/16 182287/2 SITE COMPRISES THE WHOLE OF LOT 2 ON PLAN No. SP. 182287	STRATA PLAN NEW SHEET 1 OF 3 SHEETS (PURSUANT TO E277703)		Registered Number 126707
	NAME OF STRATA SCHEME KINGSTON TOWN SHOPPING CENTRE, KINGSTON		STRATA TITLES ACT 1998 REGISTERED 6-FEB-1997 <i>Michael Price</i> Recorder of Titles
SCALE 1: 2000	LENGTHS IN METRES		
SITE PLAN			
NOTES: (i) ALL BUILDINGS ON THE SITE TO BE SHOWN ON SHEET 1. (ii) BUILDING TO SITE BOUNDARY OFFSETS OF LESS THAN 2.00 METRES TO BE SHOWN ON SHEET 1.		Council Delegate <i>[Signature]</i> Date: 21-12-21	Registered Land Surveyor <i>[Signature]</i> Date: 8-11-21
STAGED/COMMUNITY DEVELOPMENT. SCHEME No. (IF APPLICABLE)		LODGED BY LEARY COX & CRIPPS SURVEYORS	

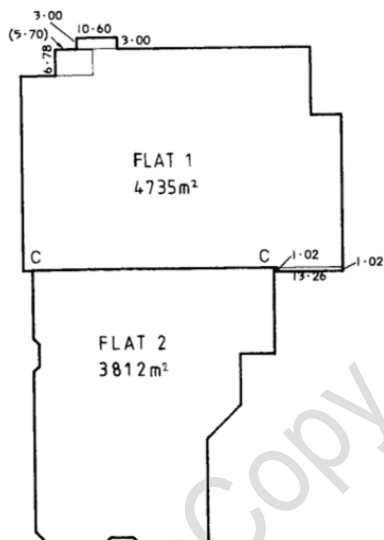
STRATUM PLAN

SHEET 2 OF 3 SHEETS

Registered Number

126707

GROUND FLOOR
SCALE 1:1000



DESCRIPTION OF BOUNDARIES
HORIZONTAL BOUNDARIES OF FLATS ARE SHOWN BY HEAVY UNBROKEN LINES.
THE BOUNDARIES OF EACH FLAT EXTEND—
A. HORIZONTALLY ALONG FACE OF BUILDING, ALONG OPEN BOUNDARIES
DEFINED BY MEASUREMENT AND ALONG BOUNDARIES MARKED CC
DEFINED BELOW AS THE CASE MAY BE.
BOUNDARIES MARKED CC ARE CENTRE OF WALL
MEASUREMENTS IN BRACKETS ARE FOR BOUNDARY FIXATION ONLY
B. VERTICALLY FROM TWO METRES BELOW GROUND LEVEL TO A HEIGHT OF
FIFTEEN METRES ABOVE GROUND LEVEL.



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



SCHEDULE OF EASEMENTS	Registered Number
NOTE: THE SCHEDULE MUST BE SIGNED BY THE OWNERS & MORTGAGEES OF THE LAND AFFECTED. SIGNATURES MUST BE ATTESTED.	SP182287

PAGE 1 OF 4 PAGES

EASEMENTS AND PROFITS

Each lot on the plan is together with:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as may be necessary to drain the stormwater and other surplus water from such lot; and
- (2) any easements or profits a prendre described hereunder.

Each lot on the plan is subject to:-

- (1) such rights of drainage over the drainage easements shown on the plan (if any) as passing through such lot as may be necessary to drain the stormwater and other surplus water from any other lot on the plan; and
- (2) any easements or profits a prendre described hereunder.

The direction of the flow of water through the drainage easements shown on the plan is indicated by arrows.

The portion of lot 1 formerly comprised in Lot 2 on SP 27282 is TOGETHER WITH a right of drainage over the area marked DRAINAGE EASEMENT 2.00 WIDE on the Plan (SP 17485)

The portion of lot 1 formerly comprised in Lot 2 on SP 27282 is TOGETHER WITH a right of drainage over the area marked DRAINAGE EASEMENT 2.50 WIDE on the Plan (SP 40746)

P

~~The portion of lot 1 marked ABCD~~ is TOGETHER WITH a Services Easement created by and more fully set forth in Transfer No E160163 over the area marked PRIVATE SERVICE EASEMENT 1.00 WIDE on the Plan

D


~~The portion of lot 1 marked ABCD~~ is TOGETHER WITH a Water Supply Easement created by and more fully set forth in Transfer No E160163 over the area marked PRIVATE SERVICE EASEMENT TO SUPPLY WATER 2.00 WIDE on the Plan

Lot 1 is TOGETHER WITH a Right of Carriage Way (as defined herein) over the area marked "RIGHT OF WAY (PRIVATE) VARIABLE WIDTH" on the Plan (SP 177320)

Lot 1 is SUBJECT TO a Right of Carriage Way as defined herein (appurtenant to lot 1 on Sealed Plan 177320) over the area marked "RIGHT OF WAY (PRIVATE) 6.50m WIDE" passing through that lot on the Plan (as defined herein) (SP 177320)

Lot 1 is SUBJECT TO a Pipeline and Services Easement in gross in favour of the Tasmanian Water and Sewerage Corporation Pty Limited, its successors and assigns ("TasWater") over the area marked "PIPELINE & SERVICES EASEMENT 3.00 WIDE" passing through that lot on the Plan ("the Easement Land")

(USE ANNEXURE PAGES FOR CONTINUATION)

SUBDIVIDER: STRATA CORPORATION NUMBER 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON FOLIO REF: 126707/0 SOLICITOR & REFERENCE: BW171918: Butler McIntyre and Butler	PLAN SEALED BY KINGBOROUGH COUNCIL DATE: 21.12.21 DAS-202-16 REF NO.  Council Delegate
NOTE: The Council Delegate must sign the Certificate for the purposes of identification.	

f:\data\affinity_docs\reve-bui-171918\preve-bui_171918_027.docx



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 2 OF 4 PAGES</p>	<p>Registered Number</p> <p>SP 182287</p>
<p>SUBDIVIDER: STRATA CORPORATION NUMBER. 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON</p> <p>FOLIO REFERENCE: 126707/0</p>	

The portion of lot 2 marked CDKJ is together with a right of carriageway over the area marked EFGH on the Plan

The portion of lot 2 formerly comprised in Lot 2 on SP 27282 is TOGETHER WITH a right of drainage over the area marked DRAINAGE EASEMENT 2.00 WIDE on the Plan

The portion of lot 2 formerly comprised in Lot 2 on SP 27282 is TOGETHER WITH a right of drainage over the area marked DRAINAGE EASEMENT 2.50 WIDE on the Plan

Lot 2 is SUBJECT TO a Right of Carriage Way ~~as defined herein~~ (appurtenant to Lot 1 on the Plan) over the area marked "RIGHT OF WAY (PRIVATE) VARIABLE WIDTH" passing through that lot on the Plan ^(SP177320)

Lot 2 is SUBJECT TO a Right of Carriage Way ~~as defined herein~~ (appurtenant to Lot 1 on Sealed Plan 177320) over the area marked "RIGHT OF WAY (PRIVATE) VARIABLE WIDTH" passing through that lot on the plan ^(SP177320)

FENCING PROVISION:

In respect of the Lots shown on the Plan the Vendor (STRATA CORPORATION NUMBER 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON) shall not be required to fence

COVENANTS:

The owner of lot 2 on the Plan covenants with the Kingborough Council to the intent that the burden of this covenant may run with and bind the covenantor's lot and each and every part of it and that the benefit of the covenant shall be annexed to and devolve with Kingborough Council to observe the following stipulation-

1. Not to develop the lot except for multiple dwellings only

DEFINITIONS:

"Pipeline and Services Easement" means:-

FIRSTLY, THE FULL AND FREE RIGHT AND LIBERTY for TasWater and its employees, contractors, agents and all other persons duly authorised by it, at all times to:

- (1) enter and remain upon the Easement Land with or without machinery, vehicles, plant and equipment;
- (2) investigate, take soil, rock and other samples, survey, open and break up and excavate the Easement Land for any purpose or activity that TasWater is authorised to do or undertake;
- (3) install, retain, operate, modify, relocate, maintain, inspect, cleanse, repair, remove and replace the Infrastructure;
- (4) run and pass sewage, water and electricity through and along the Infrastructure;

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

f:\data\affinity_docs\reve-bui\171918\preve-bui_171918_027.docx



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



<p>ANNEXURE TO SCHEDULE OF EASEMENTS</p> <p>PAGE 3 OF 4 PAGES</p>	<p>Registered Number</p> <p>SP 182287</p>
<p>SUBDIVIDER: STRATA CORPORATION NUMBER. 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON</p> <p>FOLIO REFERENCE: 126707/0</p>	

- (5) do all works reasonably required in connection with such activities or as may be authorised or required by any law:
 - (a) without doing unnecessary damage to the Easement Land; and
 - (b) leaving the Easement Land in a clean and tidy condition;
- (6) if the Easement Land is not directly accessible from a highway, then for the purpose of undertaking any of the preceding activities TasWater may with or without employees, contractors, agents and any other persons authorised by it, and with or without machinery, vehicles, plant and equipment enter the Lot from the highway at any vehicle entry and cross the Lot to the Easement Land; and
- (7) use the Easement Land as a right of carriageway for the purpose of undertaking any of the preceding purposes on other land, TasWater reinstating any damage that it causes in doing so to any boundary fence of the Lot.

SECONDLY, the benefit of a covenant in gross for TasWater with the registered proprietor/s of the Easement Land and their successors and assigns not to erect any building, or place any structures, objects, vegetation, or remove any thing that supports, protects or covers any Infrastructure on or in the Easement Land, without the prior written consent of TasWater to the intent that the burden of the covenant may run with and bind the servient land and every part thereof and that the benefit thereof may be annexed to the easement herein described.

INTERPRETATION:

“**Infrastructure**” means infrastructure owned or for which TasWater is responsible and includes but is not limited to:

- (a) sewer pipes and water pipes and associated valves;
- (b) telemetry and monitoring devices;
- (c) inspection and access pits;
- (d) electricity assets and other conducting media (excluding telemetry and monitoring devices);
- (e) markers or signs indicating the location of the Easement Land or any other Infrastructure or any warnings or restrictions with respect to the Easement Land or any other Infrastructure;
- (f) anything reasonably required to support, protect or cover any other Infrastructure;
- (g) any other infrastructure whether of a similar nature or not to the preceding which is reasonably required for the piping of sewage or water, or the running of electricity, through the Easement Land or monitoring or managing that activity; and
- (h) where the context permits, any part of the Infrastructure.

“**TasWater**” means Tasmanian Water & Sewerage Corporation Pty Ltd (ACN 162 220 653), its successors and assigns.

“**Right of Carriage Way**” has the same meaning as right of carriage way as set out in Schedule 8 of *Conveyancing and Law of Property Act 1884*.

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

f:\data\affinity_docs\reve-bui_171918\preve-bui_171918_027.docx

Handwritten initials/signature



SCHEDULE OF EASEMENTS

RECORDER OF TITLES

Issued Pursuant to the Land Titles Act 1980



ANNEXURE TO SCHEDULE OF EASEMENTS PAGE 4 OF 4 PAGES	Registered Number SP 182287
SUBDIVIDER: STRATA CORPORATION NUMBER. 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON FOLIO REFERENCE: 126707/0	

The Common Seal of STRATA CORPORATION NUMBER 126707, KINGSTON TOWN SHOPPING CENTRE, KINGSTON was hereunto affixed in the presence of:



Member Signature

[Handwritten Signature]
Member Signature

CHRYSANTHE GRISTWOOD

EXECUTED by
ZACHARIA INVESTMENTS Pty. Limited
(ACN 007 603 232) in accordance with
section 127 of the Corporations Act 2001:

[Handwritten Signature]
Director signature
Name: *ZACHARIA*

[Handwritten Signature]
Director signature
Name: *HELENA DEPIZZOL*

Member

NOTE: Every annexed page must be signed by the parties to the dealing or where the party is a corporate body be signed by the persons who have attested the affixing of the seal of that body to the dealing.

f:\data\affinity_docs\nichore\171918 pnichore_171918_021.doc

[Handwritten mark]



Submission to Planning Authority Notice

Council Planning Permit No.	DA-2024-239	Council notice date	10/07/2024
TasWater details			
TasWater Reference No.	TWDA 2024/00807-KIN	Date of response	23/07/2024
TasWater Contact	Phil Papps	Phone No.	0474 931 272
Response issued to			
Council name	KINGBOROUGH COUNCIL		
Contact details	kc@kingborough.tas.gov.au		
Development details			
Address	37-59 MARANOA RD, KINGSTON	Property ID (PID)	9278320
Description of development	Warehouses x7, Showrooms x2 & Car Parking x48 including 1 accessible car space		
Schedule of drawings/documents			
Prepared by	Drawing/document No.	Revision No.	Date of Issue
MK Design	Architectural Plans / A1.0 – A15.0	J	16/05/2024
Flussig	Sewer Servicing / H200 & Water Servicing H-300	P3	20/05/2024
Conditions			
<p>Pursuant to the <i>Water and Sewerage Industry Act 2008 (TAS)</i> Section 56P(1) TasWater imposes the following conditions on the permit for this application:</p> <p>CONNECTIONS, METERING & BACKFLOW</p> <ol style="list-style-type: none"> The proposed development must be serviced by a suitably sized water supply with a metered connection and sewerage system and connection to TasWater’s satisfaction and be in accordance with any other conditions in this permit. <i>Advice: TasWater will not supply or read individual tenancy meters unless the development is to be strata subdivided. Also refer water submetering advice below.</i> Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer’s cost. Prior to commencing construction of the development, any water connection utilised for the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater. <p>DEVELOPER CHARGES</p> <ol style="list-style-type: none"> Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$5,622.40 to TasWater for water infrastructure for 3.2 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater. Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$9,312.10 to TasWater for sewerage infrastructure for 5.3 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater. 			



DEVELOPMENT ASSESSMENT FEES

- 6. The applicant or landowner as the case may be, must pay a development assessment fee of \$403.51 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater. The payment is required within 30 days of the issue of an invoice by TasWater.

Advice

General

For information on TasWater development standards, please visit <https://www.taswater.com.au/building-and-development/technical-standards>

For application forms please visit <https://www.taswater.com.au/building-and-development/development-application-form>

Developer Charges

For information on Developer Charges please visit the following webpage - <https://www.taswater.com.au/building-and-development/developer-charges>

Water Submetering

As of July 1 2022, TasWater’s Sub-Metering Policy no longer permits TasWater sub-meters to be installed for new strata subdivision developments. Please ensure plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) reflect this. For clarity, TasWater does not object to private sub-metering arrangements. Further information is available on our website (www.taswater.com.au) within our Sub-Metering Policy and Water Metering Guidelines.

Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

Declaration

The drawings/documents and conditions stated above constitute TasWater’s Submission to Planning Authority Notice.

TasWater Contact Details

Phone	13 6992	Email	development@taswater.com.au
Mail	GPO Box 1393 Hobart TAS 7001	Web	www.taswater.com.au

OPEN SESSION RESUMES

14 PETITIONS STILL BEING ACTIONED

There are no petitions still being actioned.

15 PETITIONS RECEIVED IN LAST PERIOD

At the time the Agenda was compiled no Petitions had been received.

16 OFFICERS REPORTS TO COUNCIL

16.1 CASH-IN-LIEU OF PARKING FOR JOHN STREET MEDICAL CENTRE

File Number: **File#**

Author: **Dave Stewart, Chief Executive Officer**

Strategic Plan Reference

Key Priority Area: 2 Deliver quality infrastructure and services.

Strategic Outcome: 2.1 Service provision meets the current and future requirements of residents and visitors.

1. PURPOSE

- 1.1 This report evaluates the options of providing a discounted rate for the Ochre Medical Centre, John Street, cash-in-lieu of parking contribution.

2. BACKGROUND

- 2.1 A minor amendment to a permit was approved (DA-2023-303-A) on 14 December 2023 for the intensification of use from four to six consulting rooms at the John Street Medical Centre, 5 John Street, Kingston. A condition of the amended permit is 'prior to the use of the two (2) additional consulting rooms commencing the developer must pay a cash contribution to Council of the provision of four (4) on-site car parking spaces. The amount payable must be determined based upon the calculations provided in Kingborough Council's 'Policy 3.19: Cash-in-Lieu of Parking Policy'.
- 2.2 The Cash-in-Lieu of Parking Policy (Policy No. 3.19) provides a framework for requiring monetary contributions in place of on-site parking spaces.
- 2.3 The policy encourages efficient land use, supports urban planning goals, and provides flexibility in parking provision for developments.
- 2.4 Under this policy, a reduction of 15% is already applied to cash-in-lieu contributions to acknowledge public benefits.
- 2.5 The applicant from the John Street Medical Centre has 'appealed' against the fees and charges associated with the Cash-in-Lieu fees they have been charged in association with DA-2023-303-A

- 2.6 This appeal has been made utilising the terms of the Fee Exemptions and Reductions Policy (Policy No: 1.17).

3. STATUTORY REQUIREMENTS

- 3.1 Table E6.1 of the Parking and Access Code of the Kingborough Interim Planning Scheme sets out the number of spaces required for different land uses, including 'medical centre', where five spaces are to be provided for each person providing health services. E6.6.1 of the Parking and Access Code sets out the discretionary criteria which can be considered in assessing and approving a reduced number of parking spaces required. The criteria include having regard to 'any verified prior payment of a financial contribution in lieu of parking for the land'.
- 3.2 The Cash-in-Lieu of Parking Policy provides for the Council's powers and obligations under the Planning Scheme in respect of the requirements for cash-in-lieu contributions when on-site car parking is not provided in a proposed development.
- 3.3 The policy aligns with the Kingborough Interim Planning Scheme 2015 and the Land Use Planning and Approvals Act 1993.

4. DISCUSSION

- 4.1 The assessment of the application determined that there was a deficit of five parking spaces under the Scheme. The deficit was determined by considering the existing number of parking spaces at the Centre and the number approved for four consulting rooms at the Centre in 2010 under DA-2010-511.
- 4.2 A traffic impact assessment was submitted as part of the minor amendment application which included a car parking demand survey and noted that the use of telehealth appointments will reduce on site medical appointments and parking demand. Upon consideration of all the relevant criteria as set out in E6.6.1 of the Parking and Access Code, a reduction of one car parking space was approved leaving a deficit of four parking spaces. The assessment of the application also noted that a relaxation of parking spaces required had also been approved under the previous two permits issued for the Centre.
- 4.3 In accordance with Policy 3.19: Cash-in-Lieu of Parking the payment required for four spaces at the medical centre is \$82,110.00. This value was reviewed and confirmed by relevant planning, legal and property staff.
- 4.4 The permit holder entered a deed for staged payment in accordance with payment requirements set out in Policy 3.19: Cash-in-Lieu of Parking.
- 4.5 The proponent of this development has now appealed the charges under the Cash-in-Lieu of Parking Policy using the Fee Exemptions and Reductions Policy as the basis for this claim.
- 4.6 The applicant asserts that 25% of their services rendered are delivered via telehealth consultations which services the community while decreasing the patient parking needs and traffic congestion. This was considered as part of the minor amendment application assessment.

Policy Justification

- 4.7 This request is based on Section 6.1 of the Kingborough Council Fee Exemptions and Reductions Policy, with the waiver requested under dot point five of the clause which states that "The fee relates to the activities of an organisation based within the Kingborough Municipal area that can demonstrate a community benefit."

- 4.8 Section 5.6 of the policy states that “Amounts over \$1,500 shall be referred to the General Manager for determination.”
- 4.9 Given the significance of this request the CEO has referred the decision making directly to Council.
- 4.10 Cash-in-lieu charges were assessed using the 2015 Interim Planning Scheme, which does not explicitly account for telehealth's impact on parking needs
- 4.11 The Cash-in-Lieu of Parking Policy allows developers to pay a cash-in-lieu contribution instead of providing the required on-site parking as per the planning scheme, when deemed appropriate by the Planning Authority.
- 4.12 Contributions are based on the cost of land and construction for equivalent parking spaces, with a 15% public benefit discount.
- 4.13 Contributions are allocated to enhance public parking or related infrastructure, including acquiring land, constructing parking facilities, or improving public transport, cycling, and walking infrastructure, in line with Council's strategic plans.

Community Benefit

- 4.14 Medical services are an important service which support the wellbeing of our communities. Community sentiment is that medical services are in short supply within our municipality.

Operational Considerations

- 4.15 The [Central Kingston Car Parking Strategy \(2023\)](#) indicates:
- Long-term parking in the Kingston CBD (240 spaces) is at capacity.
 - Capacity exists within the long-term parking areas in the Kingston CBD periphery and at the external park-and rides.
 - Short-term parking demands for on-street and off-street spaces in the Kingston CBD are moderate, with around 500 available vacancies.
- 4.16 While short term capacity does exist in the Kingston CBD area, decreasing the direct provision of car parking from an individual business and not requiring a full cash-in-lieu of parking offset does shift some private responsibility to public responsibility in the long-term management of parking capacity in the municipal area.

5. FINANCE

- 5.1 Providing a discount to the cash-in-lieu contribution may reduce the immediate funds available for parking or related infrastructure development.
- 5.2 If this precedent leads to widespread discounts across multiple developments, it could limit the Council's ability to fund essential projects, potentially delaying planned parking upgrades or active transport enhancements.

6. ENVIRONMENT

- 6.1 The cash-in-lieu of parking contribution, or the reduction of it has no direct environmental impact.
- 6.2 Funding from a cash-in-lieu of parking contribution can be used for other non-private car based transport options which may have a positive environmental impact.

7. COMMUNICATION AND CONSULTATION

- 7.1 The applicant has submitted an appeal for consideration of the cash-in-lieu of parking charges that they are being charged as part of DA-2023-303-A.

8. RISK

Equity Concerns

- 8.1 Granting discounts selectively, even when justified, may raise equity concerns among other developers or businesses. Developers who meet their full on-site parking requirements or pay the standard cash-in-lieu contributions may perceive the discounted rate as preferential treatment, leading to reputational risks for the Council. This could result in challenges or disputes from other applicants.

Administrative Risk

- 8.2 Introducing discounted rates based on community benefits and reduced parking demand requires robust assessment criteria. Ambiguous or inconsistently applied criteria could lead to disputes, misinterpretation of policy intent, or inefficiencies in administration.

Environmental Risk

- 8.3 While the telehealth component of the proposal supports accessibility and with sustainability outcomes, there is a minor risk that the overall discounted rate may indirectly encourage developments to under-provide on-site parking, increasing localised congestion if demand for parking exceeds available capacity.

Precedent Risk

- 8.4 Offering discounts in this case might establish a precedent that encourages future applicants to request similar concessions. Without clear boundaries or guidelines, the Council may face pressure to extend discounts to less justifiable cases, potentially undermining the policy's primary purpose.

9. CONCLUSION

- 9.1 The applicant provides medical services, including telehealth consultations, reduce parking demand and support community health. A discounted cash-in-lieu rate aligns with the policy's objectives and broader community planning goals.

10. RECOMMENDATION

That Council provide the Ochre Medical Centre, John Street, a 25% discount on the levied cash-in-lieu of parking fee, acknowledging the community value of medical services and the telehealth services provided decreasing the on-site services being provided to patients.

ATTACHMENTS

Nil

16.2 KINGBOROUGH COMMUNITY SAFETY COMMITTEE REVIEW

File Number: 5.476

Author: Anthony Verdouw, Executive Officer Engineering Services

Authoriser: David Reeve, Director Engineering Services

Strategic Plan Reference

Key Priority Area: 1 Encourage and support a safe, healthy and connected community.

Strategic Outcome: 1.1 A Council that engages with and enables its community.

1. PURPOSE

1.1 The purpose of this report is to review the ongoing operations of the Kingborough Community Safety Committee (KCSC).

2. BACKGROUND

2.1 In late 2023 Council called for community representative nominations for KCSC. There were a low number of community nominations and no nominations from potentially new community representatives.

2.2 At its 18 December 2023 meeting Council resolved that KCSC meet over a 12-month period in 2024 and review the Committee's roles and objectives and provide clear goals and functions for the Committee going forward and that a follow up report be brought to Council in late 2024 outlining recommendations regarding the ongoing operation of KCSC.

3. STATUTORY REQUIREMENTS

3.1 KCSC is a committee appointed by Council in accordance with section 24 of the Local Government Act 1993.

4. DISCUSSION

4.1 There was limited community interest to join KCSC when nominations were called for in late 2023, with only six (6) nominations received.

4.2 It was suggested that the six (6) nominations be endorsed by Council and that the Committee's strategic outcomes and goals be reviewed with an aim to encourage more community engagement and to provide better value both for the community and Council.

4.3 Subsequently, the Committee started the 2024 term with a workshop facilitated to brainstorm ideas for improving the Committee, review the purpose of the Committee and compile a list of opportunities moving forward. For the remainder of the year the Committee met on a bi-monthly basis as per normal.

4.4 At the final KCSC meeting of the year (9 December 2024 – meeting minutes attached) the Committee reviewed achievements and opportunities for 2024 and discussed options for the Committee moving forward.

4.5 Some key achievements were noted, however, progress on a broader strategic framework and better engaging with the wider community and understanding their key safety concerns was flagged as an ongoing opportunity requiring more resources.

4.6 The Committee members suggested that Council should assess whether Council is getting value for the time and resources put into the Committee, and whether similar

outcomes could be achieved without specific input from the Committee, or similar results achieved via engagement through different forums.

4.7 The Committee recommended the below potential options for KCSC moving forward:

4.7.1 The Committee continues in 2025 with a revised Terms of Reference and a plan to address the areas for opportunity; or:

4.7.2 The Committee be disbanded, with the below suggestions for Council to continue to receive and respond to community safety concerns:

- That the Kingborough Council Community Forum be reviewed to incorporate safety related requests and also receive input from areas such as Kingston and Margate (not currently represented in the forum) and neighbourhood watch groups.
- A safety report feedback option be added to Council's website.
- That Council work with Tasmania Police and support more "See it, hear it, report it" campaigns on social media and the website.
- That Tasmania Police representatives continue to provide ongoing scheduled updates to the CEO and key Council areas and an annual presentation to the Councillors.

4.8 Council officers have reviewed the Committee's operations and recommend that without the broader strategic framework and a lack of time resources from both staff and the community representatives to implement significant strategic goals, the Committee doesn't provide sufficient value to warrant the resourcing required to run it.

4.9 Additionally, the Kingborough Community Forum provides an ideal opportunity for wider local community group representatives to raise awareness and provide feedback directly to Council on community safety matters specific to their local areas, replacing some of the input currently provided and discussed by KCSC.

5. FINANCE

5.1 If KCSC is disbanded it will free up some minor staff resourcing which could be utilised elsewhere.

6. ENVIRONMENT

6.1 There are no relevant environmental issues to consider.

7. COMMUNICATION AND CONSULTATION

7.1 Council's resolution on this matter will be communicated to the Committee members.

8. RISK

8.1 There is a risk that disbanding KCSC could be perceived as Council pulling back from engaging on community safety. It is important that any action to disband the Committee be aligned with actions for further community safety related engagement through other means and forums and a plan for continued positive working collaboration with the local Tasmania Police Division.

9. CONCLUSION

9.1 At the Council meeting held 18 December 2023 Council requested a follow up report be brought to Council in late 2024 reviewing operations of KCSC and outlining a pathway forward for the Committee.

- 9.2 In March 2024 KCSC met for a workshop to review Committee outcomes and functions. Following the workshop the Committee meet bi-monthly for a total of 5 meetings. The Committee reviewed achievements for the year at the final meeting held 9 December 2024. It was advised that Council should review the value the Committee provides and subsequently determine whether the Committee be disbanded.

10. RECOMMENDATION

That:

- (a) Council notes the minutes of the Kingborough Community Safety Committee held 9 December 2024.
- (b) The Kingborough Community Safety Committee be disbanded in 2025 and the community representatives be advised accordingly.
- (c) The below suggestions provided by the Committee be considered and progressed where possible:
 - (i) The Kingborough Community Forum be reviewed to incorporate community safety related requests and also receive input from areas such as Kingston and Margate (not currently represented in the forum) and/or neighbourhood watch groups.
 - (ii) A safety concern feedback option for the community be added to Council's website.
 - (iii) Council continues to proactively collaborate with Tasmania Police and support associated community campaigns and schedule regular meetings with the local Tasmania Police Division to address community safety matters.

ATTACHMENTS

1. KCSC Minutes 9 December 2024

MINUTES

Kingborough Community Safety Committee

Meeting No. 2024-5

Monday 9 December 2024

Kingborough

MINUTES for a Meeting of the Kingborough Community Safety Committee held at the Kingborough Council Chambers, on Monday 9 December 2024 at 2:00pm.

PRESENT

		PRESENT	APOLOGY
Chairperson	Cr Clare Glade-Wright	✓	
Deputy Chairperson	Cr David Bain	✓	
Members:	Mr Michael Brough	✓	
	Mr Jarrod Coad		X
	Ms Kate Lucas	✓	
	Mr David McLoughlin	✓	
	Mr Keith Pardoe	✓	
	Ms Colleen Ridge		X
Tasmania Police	Insp Colin Riley	✓	
Kingborough Access Advisory Committee Representative	Dr Don Hempton	✓	
Council Officers In Attendance:			
Executive Officer	Anthony Verdouw	✓	
Other Attendees:			

ACKNOWLEDGEMENT OF TRADITIONAL OWNERS

The Chairperson acknowledges and pays respect to the Tasmanian Aboriginal Community as the traditional and original owners and continuing custodians of the land on which we meet, and acknowledges elders past and present.

LEAVE OF ABSENCE

DECLARATIONS OF INTEREST

CONFIRMATION OF MINUTES

MOVED: Don Hempton
 SECONDED: Kate Lucas

That the Minutes of the Committee meeting held Monday 14 October 2024 as circulated, be confirmed.

CARRIED

GENERAL BUSINESS

1. Tasmanian Police - Crime and Traffic Statistics

Inspector Riley reported on Kingborough's crime and traffic statistics and noted:

- Four focus areas: public spaces, retail theft, recidivist offenders, traffic enforcement.
- Reduction in shoplifting and high clearance rates.
- Traffic operations focused on school zones.

Committee noted that it would be worthwhile for Council to arrange or support a program to raise more awareness on reporting to Police – 'see it, hear it, report it' campaign etc.

Noted that roadside signs have been stolen and this seems to be an ongoing issue.

Committee queried status of CCTV MoU with Council and progress to-date. Noted that the LGAT state-wide process has progressed.

2. Overpass at Firthside CCTV

Committee discussed ongoing safety concerns with items dropped onto the Southern Outlet from the overpass. Insp Riley has discussed the issue directly with the CEO.

3. Review Action Items

Reviewed and discussed open action items generated from previous Committee meetings.

4. Safety Related Service Requests

Issue raised with new Huntingfield roundabout on Channel Highway and the right of way for merging cars travelling northbound in the slip lane. Insp. Riley noted to inspect noting however that it is a State Highway.

5. Committee Review

Clare summarised feedback received from Committee members regarding Committee achievements and opportunities this year.

Achievements:

- Progressed action over Huon Highway and Leslie Road leading to the quarry
- Hosted Road Safety Week stall
- Advocated for road safety in Conningham
- Reviewed school crossings
- Contributed to the effective utilisation of security cameras
- List of further achievements in meeting actions.

Further opportunity:

- Need ways to engage in or better represent the community
- Difficult for time constrained volunteers to achieve the Committee objectives
- Terms of reference are beyond reach
- Timing of the meeting difficult for attendance
- Could create working groups to consider programs/projects/initiatives.

It was noted that the question should be asked is Council getting value for the effort put into the Committee, for example time and resources. It was agreed that Council should make a judgement on this.

Noted the below potential options for the Committee moving forward:

- A. Committee continues next year with a revised Terms of Reference and plan to address the areas for opportunity.
- B. Committee disbanded, with the below suggestions for Council to continue to receive community related concerns:
 - That the Kingborough Council Community Forum be reviewed to incorporate safety related requests, and also receive input from areas such as Kingston and Margate (not currently represented in the forum) and neighbourhood watch groups.
 - Safety report feedback option added to Council's website.
 - That council support more "See it, hear it, report it" campaigns on social media and website.
 - That Inspector Riley gives regular updates to the CEO and key Council areas and an annual presentation to the councillors.

6. Correspondence

7. Other Business

CLOSURE: There being no further business, the Chairperson declared the meeting closed at 3.15pm

Meeting Action Items						
Meeting #	Action Number	Description	Responsibility of	Due Date	Action Undertaken	Complete
2024-03	7	Anthony to liaise with RACT regarding presenting at the next Committee meeting.	Anthony Verdouw	14/10/2024	Awaiting further advice from RACT – key contact is on leave at the moment.	
Meeting Items Reviewed/Actioned						
Meeting #	Action Number	Description	Responsibility of	Due Date	Action Undertaken	Complete
2024-04	1	Council staff to follow up CCTV grant opportunities and other funding options to progress the CCTV rollout and upgrades.	Anthony Verdouw	16 Dec 24	There were some federal grant streams available recently. But couldn't find any open grant funds at the moment. Staff will continue to monitor.	✓
2024-04	2	Cr Glade-Wright to follow up with community groups and relevant organisations regarding potential funding opportunities to improve security around Kingston.	Cr Glade-Wright	16 Dec 24	Kerry Muller from Business Enterprise centre suggested grant funding would be best option and that he would speak further with the Minister.	✓
2024-04	3	Staff to investigate advisory traffic signage options on Old Station Road, warning of hazards and aimed at slowing traffic and improving safety for pedestrians.	Renai Clark	16 Dec 24	Renai hasn't finalised any decisions on this matter, as there are no standard signs addressing this issue. She has designed a concept sign which might work and will liaise directly with CALSCA.	✓
2024-03	1	Insp. Riley to follow up engagement with truck drivers and operators who travel Leslie Road and Insp. Riley and Renai Clark to attend a local community group meeting on the matter.	Insp. Riley and Renai Clark	14/10/2024	Update provided and actions in place.	✓

2024-03	2	Clare to follow up with the Mayor regards response to Police staffing letter.	Cr Glade-Wright	30/08/2024	Response from Minister received.	✓
2024-03	3	Staff to follow up Summerleas Road detour signage and speed limits through the detour.	Renai Clark	30/08/2024	Additional 40 Road Works signs were installed. 2 speed trailers also set up on the route.	✓
2024-03	4	Committee members to forward any further comments in relation to the Transport Strategy review directly to Anthony for consideration.	Committee Members	6/09/2024	No further comments received.	✓
2024-03	5	Clare to bring updated Kingston CBD bus stop designs to the next meeting.	Cr Glade-Wright	21/10/2024	Council staff met with DSG and representatives from Metro and TassieLink on Thursday 26 September at which a live trial of the interchange was conducted. Following this DSG will liaise further with bus operators to determine whether any modifications to the existing design are required.	✓
2024-03	6	Clare to follow up with Council Staff to discuss opportunities for filing incoming correspondence regarding safety issues in the municipality.	Cr Glade-Wright	14/10/2024	Not progressed due to limited resourcing.	✓
2024-2	1	Clare to raise the issue of inconsistent pedestrian crossing priorities in Central Kingston with relevant Council Officers.	Cr Glade-Wright	August 2024	Raised with Program Manager.	✓
2024-2	2	Invite Community Services officers to next Committee meeting to assist with discussions around youth services and anti-social behaviour.	Anthony Verdouw	August 2024	Carol Swards attend meeting to discuss.	✓
2024-2	3	Clare to follow up regarding national safe journey to school campaigns and available resources.	Cr Glade-Wright	August 2024	Investigated but the annual date had already passed.	✓
2024-2	4	Clare to write follow up email to the local schools regarding safe journeys to school.	Cr Glade-Wright	August 2024	letter was sent to schools and a couple of responses received which were discussed at a committee meeting.	✓

2024-2	5	Clare to liaise with Council staff regards safety themes from complaints and incoming correspondence.	Cr Glade-Wright	August 2024	Asked about themes but was informed it was not possible to sort in ECM.	✓
2024-1	1	Cr Glade-Wright to follow up with staff regarding the possibility of an online survey (prepared by Committee members) and details around the proposed social media post	Cr Glade-Wright	June 2024	No.	✓
2024-1	2	Cr Glade-Wright to draft letters and media content for Committee review.	Cr Glade-Wright	June 2024	Complete and letters sent to schools - 1/5/24	✓
2024-1	3	Inspector Riley to provide feedback on enforcement on Leslie Road to next Committee meeting following the reinstatement of the 60km/h speed limit signage.	Inspector Riley	June 2024	Feedback provided to the Committee	✓
2024-1	4	Cr Glade-Wright to ask Communications Advisor if a brochure on the Committee can be prepared to share with community members on the day.	Cr Glade-Wright	June 2024	Didn't happen due to resourcing.	✓
2024-1	5	Renai to provide advice on Old Station Road / Coningham Road intersection give way priorities.	Renai Clark	June 2024	Works will be planned in the summer months to change the priority.	✓
2023-5	1	Cr Glade-Wright to liaise further with Council staff regarding TasNetworks approval for the CCTV camera on TasNetworks pole at Blackmans Bay Beach.	Cr Glade-Wright	December 2023	Staff continuing discussions with TasNetworks.	✓
2023-5	2	Inspector Riley to follow up traffic enforcement in the Leslie Vale area and report back to the Committee.	Inspector Riley	December 2023	No heavy vehicles were detected exceeding the 40km/h 10 tonne and over speed limit on Leslie Road. There were only a small number of standard passenger vehicles detected exceeding the 70km/h speed limit.	✓
2023-4	1	Cr Glade-Wright to follow up if concerns around the Huon Highway/Leslie Road intersection can be elevated on behalf of KCSC and potentially a meeting be arranged with the Mayor and State Minister to discuss the issues.	Cr Glade-Wright	16/10/23	Mayor sent follow up letter to the Minister and the minister has replied, per meeting Correspondence	✓

2023-4	2	Staff to follow up with State Growth if there is any stakeholder input regarding the placement of the new traffic cameras.	Anthony Verdouw	16/10/23	State Growth advised the there is a form for recommendations for new speed camera locations.	✓
2023-3	1	Council staff to contact Leslie Vale Quarry requesting information on heavy vehicle volumes accessing the site.	Renai Clark	21/8/23	See below.	✓
2023-3	2	Council staff to install traffic counters on Leslie Road west of the Huon Highway intersection to collect traffic data.	Renai Clark	21/8/23	Traffic counters have been scheduled for installation for the last week in August and first week in September. We will provide traffic data for the October meeting. Going to put both sides of Highway now, instead of contacting quarry, so we can measure passenger vehicles and speeds as well.	✓
2023-3	3	Councillor Glade-Wright to follow up potential Kingborough Chronicle article outlining what information to provide when contacting police.	Cr Glade-Wright	21/8/23	Article published in the Chronicle on 15 August 2023	✓
2023-3	4	Inspector Riley to follow up regarding security cameras in the area and report back.	Insp. Riley	21/8/23	Insp. Riley provided an update on security cameras in the Tasmania Police crime and traffic report.	✓
2022-3	4	Staff to follow up why yellow no parking lines have not been reinstated at Village Drive intersection.	Works Depot	15/8/22	The linemarking has now been completed.	✓
2021-6	2	Investigate northern end of Auburn Rd and assess whether extending yellow no parking lines are warranted. Staff to investigate and assess if extending yellow no parking lines are warranted at the Church Street end of Auburn Road.	Renai Clark	20/02/23	The linemarking has now been completed.	✓
2023-1	1	Staff to write to State Growth to clarify responsibilities for operation and maintenance of lights in the underpass at the Summerleas/Channel Hwy (KFC) Roundabout, noting TasPolice also have concerns with lack of lighting at underpass.	Anthony Verdouw	17/4/23	State Growth have replied indicating they maintain and manage all lighting in pedestrian underpasses on State roads or highways. Their maintenance contractor repairs outages and has been notified to	✓
					address any issues at the KFC roundabout.	

Kingborough Community Safety Committee

Meeting Dates for 2024

*Meetings are where possible held on a Monday every second month at 2pm
in the Council Chambers, Kingston*

29 April

24 June

19 August

14 October

9 December

Public Copy

17 NOTICES OF MOTION

17.1 INFRASTRUCTURE PROJECTS

The following Notice of Motion was submitted by **Cr Deane**:

RECOMMENDATION

That Council:

1. Conduct a review of its handling of recent infrastructure projects, including:
 - Kingston bus interchange
 - Summerleas Road underpass
2. Provide a report to Council summarising the review's key findings.

Background

Each year, Council delivers infrastructure projects throughout Kingborough. While the majority of these are delivered on time, the bus interchange in Kingston & Summerleas Road underpass projects both continue to experience significant delays.

Moving forward, the demand on Council to successfully deliver major infrastructure projects will only increase following the state government's decision to locate the JackJumpers' and Devils' new high performance centres within the Kingston sports precinct. Council will not only play a role in these projects being delivered but will also require Council to manage several other associated projects e.g. construction of additional ovals, relocation of mountain bike and pump tracks, sealing of Gormley Drive etc.

As a result, it's important that Council improves how it manages major projects so they are more likely to be delivered on time, within budget and in line with community expectations.

The intention of this motion is to ensure a review is undertaken and that its key findings are made available to help restore the community's confidence in Council's capacity to manage and deliver major projects.

Officer's Response

Council acknowledges the value to continuous improvement, and learning from previous projects is a critical element of this. A scope for the review of the Summerleas Road Underpass and the Kingston Bus Interchange projects has been developed, and a consultant has been selected for this work. A summary of the key findings from this review can be brought back to council.

Dave Stewart, Chief Executive Officer

18 CONFIRMATION OF ITEMS TO BE DEALT WITH IN CLOSED SESSION

RECOMMENDATION

That in accordance with the *Local Government (Meeting Procedures) Regulations 2015* Council, by absolute majority, move into closed session to consider the following items:

Confirmation of Minutes

Regulation 34(6) *In confirming the minutes of a meeting, debate is allowed only in respect of the accuracy of the minutes.*

Applications for Leave of Absence

Regulation 15(2)(h) *applications by councillors for a leave of absence*

Tender Assessment – AB2407 – Civic Centre HVAC System Upgrade

Regulation 15(2)(d) *contracts, and tenders, for the supply and purchase of goods and services and their terms, conditions, approval and renewal.*

Closure of part of the LGA Subdivision Road ('intersecting Channel Highway & Pin Oak Place') comprised in folio of the Register Volume 160038 folio 100

Regulation 15(2)(f) *proposals for the council to acquire land or an interest in the land or for the disposal of land.*

In accordance with the Kingborough Council *Meetings Audio Recording Guidelines Policy*, recording of the open session of the meeting will now cease.

Open Session of Council adjourned at

OPEN SESSION ADJOURNS

OPEN SESSION RESUMES

RECOMMENDATION

The Closed Session of Council having met and dealt with its business resolves to report that it has determined the following:

Item	Decision
Confirmation of Minutes	
Applications for Leave of Absence	
Tender Assessment – AB2407 – Civic Centre HVAC System Upgrade	
Closure of part of the LGA Subdivision Road ('intersecting Channel Highway & Pin Oak Place') comprised in folio of the Register Volume 160038 folio 100	

CLOSURE

Public Copy

APPENDICES

- A Chief Executive Officer's Activities 4 November 2024 to 20 December 2024
- B Infrastructure Works Report September 2024 to December 2024
- C Audit Panel Minutes 6 December 2024
- D Kingborough Bicycle Advisory Committee Minutes 13 Dec 2024
- E Complaints Lodged with Council 1 October 2024 to 31 December 2024
- F Councillor Attendance at Meetings and Workshops 1 October 2024 to 31 December 2024
- G Councillor Allowances & Expenses 1 July 2024 to 31 December 2024
- H Donations Table 1 July 2024 to 31 December 2024

Public Copy

A CHIEF EXECUTIVE OFFICER'S ACTIVITIES 4 NOVEMBER 2024 TO 20 DECEMBER 2024

Date	Description
4 – 7 Nov	Sick leave
7 November	Met with Alicia McKay
11 November	Attended Councillor workshop
13 November	Attended Greater Hobart Committee Meeting in company with the Mayor
	Met With Danielle Reid with the Mayor
	Attended the JackJumpers High Performance Centre Steering Committee meeting
14 November	Met with Matthew Snow of KPMG
	Met with members of the Kingston Beach Surf Lifesaving Club
	Met with representatives of MS Civil
	Attended the Southern GM/CEO's Climate Capability Program workshop
15 November	Met with representatives of Christian Homes Tasmania re: Pinnacle Development
	Attended the Derwent Estuary Program AGM
	Attended the International Men's Day Event
18 November	Met with representatives of PDA Surveyors
	Attended Council meeting
19 November	Attended AFL High Performance Training Centre media event with the Mayor
	Participated in Metropolitan Council GM's/CEO's weekly meeting
	Attended TasWaste South 'Regional Approach to Organics' meeting
20 November	Met with representatives of Homes Tas
21 November	Attended the LGAT General meeting
22 November	In company with the Mayor, met with Nic Street MP
25 November	Attended Councillor workshop
27 November	In company with the Mayor, attended the opening of the Kingborough Helping Hands Giving Tree
	Attended the Greater Hobart GM/CEO's General Meeting
28 November	Attended the Kingborough Community Forum meeting
29 November	Met with representatives of Kingston Neighbourhood House
2 December	Attended Council meeting
3 December	Participated in Metropolitan Council GM's/CEO's weekly meeting
4 December	Attended the SETN Board meeting
5 December	Met with representatives of the Department of State Growth on the AFL High Performance Training Centre
6 December	Attended Audit Panel meeting

Date	Description
	Attended a regional collaboration meeting with other Council CEO's and Regional Development Australia
9 December	Attended Councillor workshop
12 December	Presented to the Tasmanian Community Fund Board
16 December	Attended Council meeting
17 December	Participated in Metropolitan Council GM's/CEO's weekly meeting

Public Copy

B INFRASTRUCTURE WORKS REPORT SEPTEMBER 2024 TO DECEMBER 2024

File Number: 25.9

Author: Anthony Verdouw, Executive Officer Engineering Services

Authoriser: David Reeve, Director Engineering Services

Contracted Capital Projects

1. Blowhole Road Reconstruction:

The reconstruction of Blowhole Road was completed by Duggans Pty Ltd in November 2024 and is now open for public use. The project included the installation of new kerbs and gutters, footpaths, driveways, and stormwater system, enhancing safety and accessibility for local residents and visitors to Blackmans Bay Beach.



2. Pelverata Road (Vic 40) Realignment:

Contract for project has been awarded to Crossroads Civil Construction. TasNetworks have recently completed the relocation of pole infrastructure to enable the reconstruction of the road. The pole relocation necessitated vegetation clearing, to comply with TasNetworks standards and the required alignment of the road. Site works are set to begin in late January 2025.



3. Kingston Beach Change Room and Carpark:

The new change room construction is currently close to completion at Kingston Beach Oval. Internal painting and features installation are underway as well as electrical and plumbing works. Carpark upgrades will start shortly.



4. Channel Highway (Vic 157-197) Kingston Footpath Construction:

The footpath construction along Channel Highway in Kingston was completed by Crossroads Civil Construction in December 2024. The project included the construction of concrete footpaths on both sides of the highway, pedestrian refuge, installation of kerbs and gutters, stormwater system upgrades, and the addition of a new bus bay.



5. Woodbridge Oval Carpark Upgrade:

JRV Civil Contracting Pty Ltd has completed the car park upgrades at Woodbridge Oval, providing 48 gravel parking spaces, including two designated accessible parking spots.



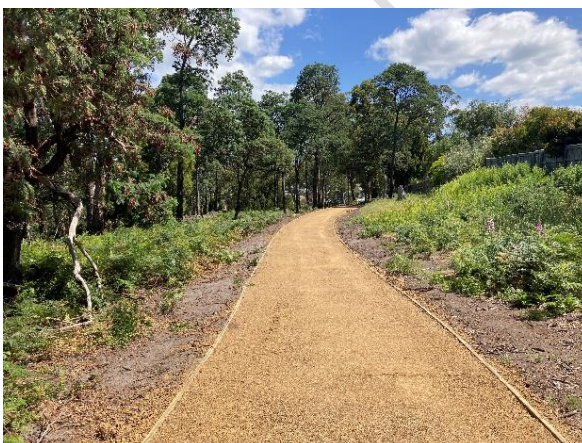
6. Maranoa Road-Denison Street Intersection Reconstruction:

The intersection upgrades for Maranoa Road and Denison Street are being programmed for completion during the Christmas/New Year holiday period, due to the proximity of local schools. This will enhance safety and minimise traffic disruptions whilst work is underway.



7. KSC to Whitewater Creek Connector Track Stage 2:

Construction of the KSC to Whitewater Creek Shared Path (Stage 2) began in November 2024. StateWide has completed the shared path and stairway, while AJR Construction finished the pier and abutment works in December 2024. Footbridge construction is ongoing, with truss installation scheduled to begin in late January. Once completed, this project will provide pedestrian access between the Sports Centre and Spring Farm/Whitewater Park Estate, with further connection to Kingston Park upon the completion of Summerleas Underpass.



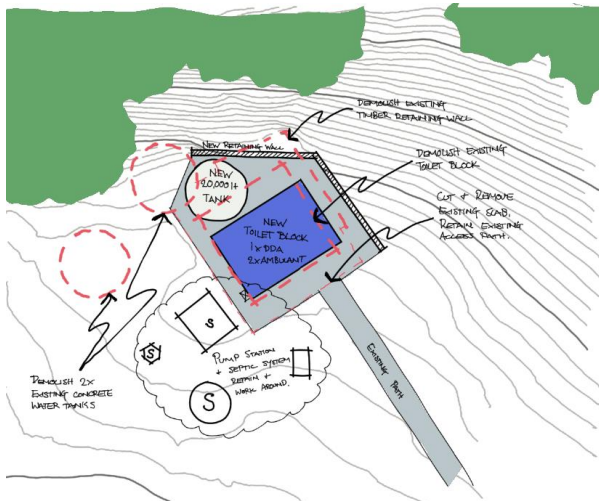
8. Trial Bay Foreshore Toilet Replacement:

Following the evaluation process, the contract has been awarded to Straight Up Building TAS Pty Ltd (TAS Built). The project involves demolishing the existing toilet block and replacing it with a new facility, which will include one accessible cubicle and two unisex ambulant cubicles. The toilet kit has been ordered and is currently being manufactured on the mainland. Construction will begin once the kit arrives.



9. Silverwater Park Toilet Replacement:

The contract for replacing the toilet block at Silverwater Park has been awarded to JMK Construction Pty Ltd. The project includes demolishing the existing facility and installing a new facility featuring one accessible cubicle and two unisex ambulant cubicles. The toilet kit has been ordered and is currently under production, with site works set to commence once the kit arrives.



10. Silverwater Park Upgrade:

The tender evaluation has been completed with the contract awarded to AJR Construction. The project includes building a new playground and a raised FRP walkway to provide DDA-compliant access from the roadway to the playground and park facilities. Work is expected to begin in March 2025.



Works Department – Works Recently Completed (Mainland Kingborough)

11. Brook Lane, Margate (Capital) – reseal prep works completed:



12. Baynton Street, Kingston (Capital) – reconstructed the link footpath from Baynton Street to Bowral Court:



13. Pelverata Road – major pavement repairs:



14. Amarina Court, Kingston Beach – asphalt patching:



15. Huntingfield – stormwater pipe extension:



16. Stormwater lines blasted over the last couple of months:

- 135 Sandfly Road, Sandfly
- 4 Beach Road, Margate
- 12 Nautilus Drive, Kingston
- 38 Nolan Crescent, Kingston
- Dru Point, Margate
- 1/18 Blowhole Road, Blackmans Bay
- 22 Utiekah Drive, Tarooma

17. Van Morey Road, Margate (Capital) – culvert installation and sight distance improvements:



18. Drain cleaning in the following locations:

- | | | |
|-------------------------|-------------------|-------------------------|
| ▪ Manuka Road | ▪ McDowall Street | ▪ Bullock Drive |
| ▪ Watsons Road | ▪ Oates Road | ▪ Channel Highway, Snug |
| ▪ Cripps Road | ▪ Turnbulls Road | ▪ Dromana Drive |
| ▪ Thomas Road (Capital) | ▪ Websters Road | ▪ Van Morey Road |
| ▪ Fleurtys Lane | ▪ Risby Road | ▪ Fergusson Avenue |
| ▪ Slab Road | ▪ Llantwit Road | ▪ Tinderbox Road |
| ▪ Rainbirds Road | ▪ Coxs Road | |
| ▪ Honeys Road | ▪ Gallaghers Road | |
| ▪ Sunny Banks Road | ▪ Devlyns Road | |

19. Grading in the following locations:

- | | | |
|-------------------|------------------|------------------|
| ▪ Cemetery Road | ▪ Wolfes Road | ▪ Bullock Drive |
| ▪ Rainbirds Road | ▪ Jindalee Drive | ▪ McKenzies Road |
| ▪ Gormley Drive | ▪ Andersons Road | ▪ Van Morey Road |
| ▪ Summerleas Road | ▪ Besters Road | |
| ▪ Proctors Road | ▪ Clarks Road | |

20. Potholing in the following locations:

- Clare Street
- Allens Rivulet Road
- Sandfly Oval
- Besters Road
- Clarks Road
- Hovingtons Road
- Wiggins Road
- Bundalla Road
- Moodys Road
- Pregnells Road
- Leslie Road
- Cranes Road
- Stubbings Street
- Whittons Road
- Betts Road
- Tabors Road
- Cawthorn Road
- Aberys Road
- Albert Road
- Daly Road
- Sandfly Oval
- Wolfes Road
- Old Bernies Road

21. Gallaghers and Saddle Roads – sinkhole repairs.

22. Woodbridge Oval – inspected and removed dead limbs from two Stringy Bark trees:



23. Dru Point Reserve – remove three dead Pine trees:



24. 271 Roslyn Avenue, Blackmans Bay – removed significant tree after storm damage:



25. Royce Thomson Track, Boronia Hill Upgrade (Capital) – upgrade of track including vegetation clearing and construction of small flights of steps to improve user safety:



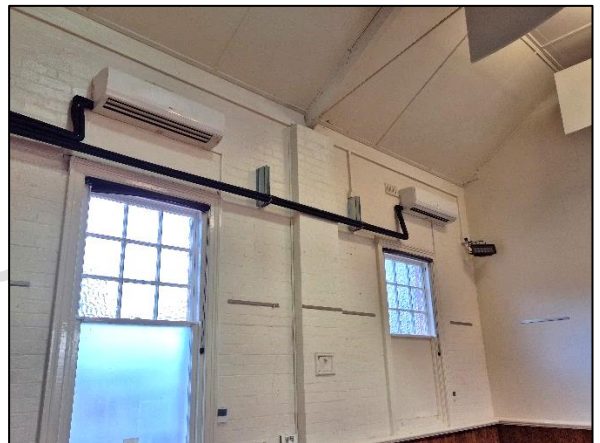
26. Blackmans Bay Hall – upper roof steel work repairs and rust removal completed. Rustproof epoxy repainting to upper section:



27. Kingston Beach Hall (Capital) – roof repairs. New colourbond roofing, gutters, fascias and skylights



28. Kingston Beach Hall (Capital) – new HVAC system installed which replaced dysfunctional gas heaters:



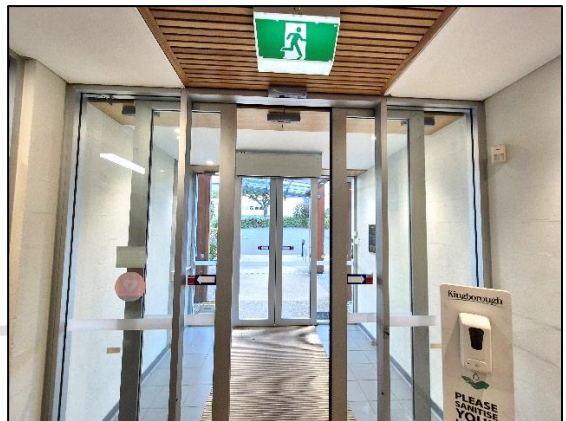
29. Woodbridge Hall – sub-floor structural repairs. New Tas Oak timber overlay with a 2-pack epoxy wear coating:



30. Middleton Hall – new Tas Oak timber overlay with 2-pack epoxy wear coating:



31. Civic Centre – maintenance upgrade on front and back entrance doors. Mechanisms are DDA compliant:



Works Department – Works Recently Completed (Bruny Island)

32. Adventure Bay Road – asphalt patching undertaken by Contractor:



33. Maintenance grading undertaken on the following roads:

- Blinkbonny Road
- Lockleys Road
- Resolution Road
- Ritchie Street
- Cemetery Road
- Pontoon Road
- Simpsons Bay Road
- Lighthouse Road
- Wooreddy Road North
- Musketts Road
- Matthew Flinders Drive
- Cloudy Bay Road, PWS PWA014

34. Wisbys Road – guidepost replacement.
35. Adventure Bay Road – shoulder reinstatement.
36. Adventure Bay Road and Lockleys Road – tree removal.
37. Cloudy Bay Road (Capital) – drain cleaning.

Works Department – Works Underway / Planned (Mainland Kingborough)

38. Thomas and Leslie Road (Capital) – resheeting.
39. Leslie Road (Capital) – culvert installation.
40. Upcoming Two-Coat Reseals:
 - Hackford Drive
 - Corbys Road
 - Fergusson Avenue
 - Rada Road
 - Brook Lane
 - Pelverata Road

Works Department – Works Underway / Planned (Bruny Island)

41. Cloudy Bay Road – red gravel resheeting.

Public Copy

C **AUDIT PANEL MINUTES 6 DECEMBER 2024**



**AUDIT PANEL
MINUTES**

6 December 2024

MINUTES of the Kingborough Council Audit Panel held at the Council Chambers on Friday, 6 December 2024 at 8.00 am.

PRESENT:

		PRESENT	APOLOGY
Chair	Mr P McTaggart	Y	
	Mr P Viney	Y (online)	
	Ms C Millar	Y	
	Cr D Bain	Y	
	Cr A Antolli		Y

IN ATTENDANCE

Councillor F Fox	Mr D Stewart
Chief Executive Officer	Mr D Spinks
Director People and Finance	Mr T Jones
Manager Finance	Ms A Leis, Ms S McDonald (both online)
WLF Internal Auditors	Mr D Bond (online)
Tasmanian Audit Office	

WELCOME:

The Chair welcomed the Panel and Cr Fox.

APOLOGIES

Cr Antolli

EXTERNAL AUDIT

Note that the external audit agenda item was brought forward in the meeting for discussion.

Mr Bond spoke to the Memorandum of Audit Findings (MOAF). The recommendation for a judgements, estimates and assumptions paper (low risk) is considered a better practice recommendation. Two matters (one medium risk, one low risk) were raised in relation to capital work in progress (CWIP). The Panel discussed what might be a reasonable level of CWIP and CWIP completion times.

Mr Bond advised that the TAO’s contract with Crowe had reached its end and the audit would be returning to the TAO in the 2025 financial year.

Mr Bond left the meeting at 8.13am.

DECLARATIONS OF INTEREST

Independent Panel members updated Register of Interests declarations were circulated out of session prior to the meeting. The declarations were received and noted.

CONFIRMATION OF MINUTES

The Panel endorsed the minutes of the Kingborough Council Audit Panel meeting of 11 October as a true and correct record.

ACTION LIST

Action List

The Action List containing four items was noted. All four items were listed as agenda items.

INTERNAL AUDIT

Ms Leis and Ms McDonald from WLF joined the meeting online at 8.40am.

Capital Work in Progress internal audit scoping document

Ms Leis spoke to the review of CWIP management from project works finalisation to project capitalisation.

Financial Sustainability internal audit scoping document

Ms Leis spoke to the high level review of the long term financial plan including the key processes and assumptions that underpin the plan.

There were no amendments to either document. It was noted the final reports for the CWIP and the Financial Sustainability Internal Audits are to be presented to the May and February Audit Panel meetings respectively.

Internal audit status report

The Panel noted the status report of the 2024/25 internal audit program. It was resolved the follow up of closed internal audit recommendations be deferred.

WLF left the meeting at 8.51 am.

Internal Audit Recommendations Tracker

The Panel reviewed the action items under the internal audit recommendation tracker. There was discussion around the status of the Manager IT position and Mr Stewart advised the position was now filled. The Chair asked about the coming bushfire season and Council's preparedness. Mr Stewart spoke to an internal simulation exercise that had been undertaken with SES. Cr Bain enquired about progress on the strategic asset management plan (SAMP). Mr Stewart advised the SAMP is an important document, but that there was a single person dependency (an executive member) and would prefer the SAMP to be more advanced. In support, a new role had been created in the Infrastructure team to assist with overall departmental management. Mr Stewart advised single person dependencies were unfortunately common across the executive team.

EXTERNAL AUDIT

TAO Recommendations Tracker

The Panel noted the tracker update containing one item, uncapitalised WIP, which is a separate agenda item.

KEY MATTERS – COUNCIL MEETINGS OVERVIEW

Mr Stewart spoke to:

- Many initiatives and projects happening in the organisation which is stretching resources.
- Tasmanian Planning Scheme and Local Provisions Schedules (LPS). Council's LPS exhibition period commenced 9 October and closes December. Council has had a variety of engagement options for the community. A large number of submissions have been received. Council has 60 days to review the submissions and report to Council with any

recommended changes. Many of these will be complex to review as they will be technical in nature.

- A AFL High Performance Centre that has been announced for Kingborough. This will be a high profile project with risks to Council. It will be important that governance structures are appropriate, the Heads of Agreement properly drafted, and project management done well. The project is state government and AFL funded but it is important there are no surprises to council.
- Kingston Park precinct development update
- Summerleas Road underpass project – the road will be reopening December 17. This project, together with the Channel Highway bus interchange project will undergo external peer review to identify improvements and learnings.
- Executive team planning in relation to business improvement projects.
- Council annual general meeting to be held Saturday 7 December.

The Chair enquired about the Future of Local Government Review. Mr Stewart advised the priority reform program will be implemented over time.

RISK MANAGEMENT & BUSINESS CONTINUITY UPDATE

Risk management

The risk management update report was noted. The Chair enquired whether the updated risk matrix would be available for the next meeting. Mr Stewart expressed other work would likely take higher priority such as responding to submissions received in relation to the Tasmanian Planning Scheme as well as budget and financial plan. Mr Viney enquired whether there was sufficient ownership of business risks by managers. Mr Stewart advised that increased ownership and accountability was needed in order to support the executive team. In relation to support levels Ms Millar enquired about resource planning. Mr Stewart advised budget needs to be considered as well as opportunities for process and efficiency improvements.

IT Security and Policy Breaches

Nil breaches to report.

REGULATORY/GOVERNANCE UPDATES

Legal claims

The Panel noted the status report of outstanding legal claims.

Audit Panel annual performance survey

The Panel noted the results of the Panel's annual performance survey which in overall terms showed a very positive result similar to prior year. Within the overall result there were some variances from prior year.

Fraud Action Plan

The Panel noted the fraud action plan and actions undertaken during the year.

Register of Interests

Independent Panel members' updated Register of Interests declarations were considered at the start of the meeting – see Declarations of Interest.

Audit Panel Charter

The Audit Panel Charter was tabled for review. There have been no changes to the LGAT model Charter. The Panel did not consider any changes were required to its Charter.

Capital Work in Progress (CWIP) update

A CWIP status report was tabled in the meeting showing a reduction in the balance to \$26.1M from \$38.4M at the start of the financial year. A further \$11.5M, comprising mainly Kingston Park works, is ready to be capitalised. An ongoing focus will be to ensure completed projects are closed out and capitalised more promptly.

Financial report October 2024

The Panel noted the October finance report tabled at Council in November.

Audit Panel meeting dates 2025

Proposed meeting dates for 2025 were tabled. These were accepted.

Annual Work Plan

The Panel’s annual work plan for 2025 was tabled for review. The Panel agreed to some minor changes to the timing of some financial statements items brought forward, as well as the inclusion of a risk workshop with Council.

OTHER BUSINESS

Nil.

ACTION LIST

Meeting	Item	Responsibility	Due Date
Oct 2024	CWIP update to be provided at each meeting	Manager Finance	Each meeting
Oct 2024	Management presents an updated risk matrix and consider looking at getting external help with development of risk management at the Council.	Director People and Finance	May 2025
Dec 2024	Update annual work plan – timing of financial statements supporting processes brought forward and inclusion of risk workshop with Council.	Director People and Finance	Next meeting

CLOSURE

There being no further business the Chair declared the meeting closed at 10.09 am.

Confirmed as a true and correct record:

.....
Chair,
Audit Panel

D KINGBOROUGH BICYCLE ADVISORY COMMITTEE MINUTES 13 DEC 2024

File Number: 28.114

Author: Anthony Verdouw, Executive Officer Engineering Services

Authoriser: David Reeve, Director Engineering Services

Minutes

Kingborough Bicycle Advisory Committee

Meeting No. 2024-6

Friday 13 December 2024

Kingborough

MINUTES for a Meeting of the Kingborough Bicycle Advisory Committee held at the Kingborough Civic Centre, Kingston, on Friday 13 December 2024 at 9:00am.

PRESENT

		PRESENT	APOLOGY
Chairperson	Cr Amanda Midgley	✓	
Deputy Chairperson	Cr David Bain	✓	
Members:	Mark Donnellon	✓	
	Joyce du Mortier	✓	
	Emlyn Jones		X
	Kelvin Lewis	✓	
	David McQuillen	✓	
	Rob Sheers		X
	Peter Tuft	✓	
	Angela Wilson		X
Cycling South	Mary McParland		X
Bicycle Network	Alison Hetherington	✓	
Council Officers In Attendance:			
Executive Officer	Anthony Verdouw	✓	
Recreation Officer	Su Sprott	✓	
Other Attendees:			
Urban Mobility Planner – DSG	Dustin Moore		X

ACKNOWLEDGEMENT OF TRADITIONAL OWNERS

The Chairperson acknowledged and paid respect to the Tasmanian Aboriginal Community as the traditional and original owners and continuing custodians of the land on which we meet and acknowledged elders past and present.

LEAVE OF ABSENCE

DECLARATIONS OF INTEREST

There were no declarations of interest.

CONFIRMATION OF MINUTES

MOVED: Kelvin Lewis
 SECONDED: Joyce du Mortier

That the Minutes of the Committee meeting held on Friday 18 October 2024, as circulated be confirmed.

GENERAL BUSINESS1. Cycling Strategy – Strategic and Advocacy Actions Review

Review and discussion of Strategic and Advocacy Actions – Appendix 1

Action 3 – Track counters:

- Permanent counter installed on Whitewater Track. The others will be moved around to various tracks regularly.

Action 5 & Action 12 – Road safety audits to eliminate hazards for cycling – maintain existing network.

- KBAC members are invited to report on specific issues and can use Snap Send Solve or raise service requests out of session.

Action 7 & 8 – Promote safe cycling and places to ride in Kingborough.

Action 10 – Wayfinding strategy – signage and mapping

- The Kingborough Cycling Strategy Map has been updated to include completed works.
- Su rolling out more wayfinding on the Whitewater Creek Track. Noted wayfinding plan should include signage from the Kingston CBD.

Action 11 – Positive Provisioning Policy

- The Active Transport Positive Provision Policy was endorsed by Council on 21 October 2024.

2. Cycling Strategy – Infrastructure Actions Review

Review and discussion of Infrastructure Actions – Appendix 1

Action 15 – Channel Hwy Sealed Shoulders (Huntingfield to Margate section – State Growth projects)

- Noted by the Committee as a high priority safety issue for commuter and recreational cyclists.

Action 17 – Channel Trail –Margate to Huntingfield.

- [Margate to Huntingfield Shared Pathway Development Report](#).
- Keep project on the agenda and continue discussion regards next steps.
- Suggested stage project and that State and Council pursue grant funding for further design.

Action 18 – Algona Road Shared Path Feasibility Study

- Funding secured for feasibility study and concept design to be undertaken this Financial Year.
- Tenders have been received for the feasibility study.
- Noted that KBAC should provide feedback, suggestion for on-site walk though if agreeable with the consultants.

Action 20 – Huntingfield Park & Ride Connections

- Subdivision application for Huntingfield House property lodged.

Action 21 – Channel Hwy Taroona - bike Lane upgrades

- Designs are complete. Going through preliminary approvals.

Action 22 – Spring Farm to Sports Precinct

- Works for Stage 2 Spring Farm to Sports Precinct connector are well underway.
- Bridge going in early in the new year. Next year site visit or launch in the area for KBAC.

Action 23 – Huntingfield to Kingston CBD – Channel Hwy

- Spring Farm to Mertonvale footpaths – construction is nearing completion.

Action 24 – Roslyn Ave design

- Concept plans for uphill bike lanes and cost estimate presented to the Committee. Project has site constraints and a high-cost estimate.
- Committee have requested that the project remains on the 5-year plan as it is a high-priority high-use cycling link.
- Pass Cyclists Safely signage has been installed.

Action 26 – Channel Trail – Snug to Lower Snug

- Old Station Rd and Davies Rd, Lower Snug – shared path. Site survey has been completed to inform detailed designs.
- Channel Highway, Snug footpath – between Sunsail St and Snug Tiers Rd – detailed designs for works are being prepared.

Action 28 – Sandfly Road Sealed Shoulders

- Grant funding under Safer Rural Roads Program successful – sealed shoulders on Sandfly Rd focussing on priority uphill sections.
- Survey and design for works to commence soon.

3. Cycling South Report

Cycling South report on activities:

- Cycling South had a new Chair appointed at the AGM, Cr Ryan Posselt. He replaces Cr Amanda Midgley who was the previous chair for two years. A big thank you to Amanda for her time and enthusiasm filling the role.
- Tasman Bridge – A letter has been sent to both the Minister for Transport and the Minister for Infrastructure and a meeting is scheduled for early in the new year with the Minister for Infrastructure. The Mercury published an article on 16 November outlining concerns about the decision to not widen the pathways at all.
- Kingston bypass and Algona Road interchange – a public works hearing was held on 13 November which confirmed that a pedestrian overpass of the Channel Hwy was included in the design, but no funding has been allocated to construct it. The design will be available to view on the website from 9 December. https://www.transport.tas.gov.au/roadworks/current_projects/south_road_project/s/kingston_bypass_duplication_and_algona_roundabout
- Consultation is underway for a 20-year Preventative Health Strategy. The strategy identifies "activities to create enabling environments that promote health. This is through encouraging healthy behaviours such as physical activity". Cycling South supports creating safe and attractive spaces for active travel through the implementation of the Greater Hobart Cycling Plan. You can provide feedback in a

quick survey. <https://engage.stategrowth.tas.gov.au/tasmanian-walk-wheel-ride-strategy/have-your-say>.

CORRESPONDENCE

OTHER BUSINESS

4. DSG Update

Dusty was an apology, but it was noted that Tactical Cycleways Guide has been sent to Councils for feedback.

5. Kingborough Integrated Transport Strategy

Council has endorsed the draft transport strategy for community feedback early in the new year.

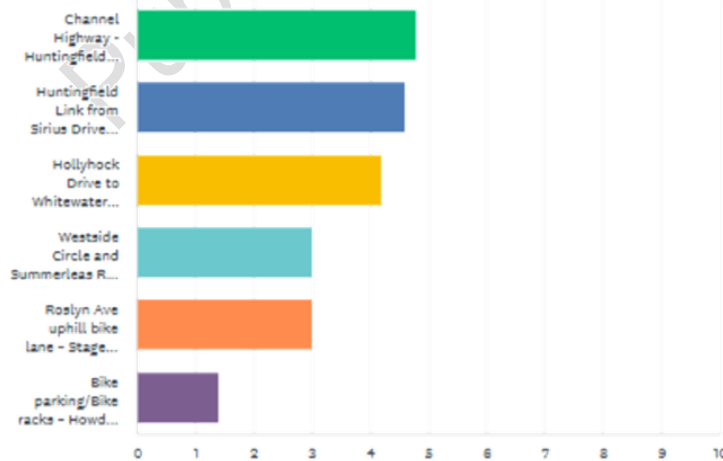
Noted that the Committee would appreciate opportunity to comment on the strategy in new year. Recommended dedicated officer for integrated transport or active transport.

6. Capital Project Bids 2025-2026

Results of the capital works project bid poll are below. Staff have raised capital project bids for design for the Huntingfield to Sirius Dr Link Path and the Hollyhock Dr to Whitewater Creek Track link path.

Please rank the below potential project bids in order of priority

Answered: 5 Skipped: 0



MATTERS OF GENERAL INTEREST

7. KBAC terms

The end of December brings to a close this 2-year term of KBAC. The Chair thanked all Committee members for their contribution over the last 2 years, in particular Peter, Joyce, Angela and Emlyn who are leaving the Committee. New nominees for the Committee will be endorsed at the 16 December Council meeting.

Noted many achievements over the year, including:

- Roslyn Ave advocacy for improvements - signs installed.
- Taroona Bike Lanes progressing.
- Sandfly Road shoulders walkthrough.
- Consistency of staff working with the Committee.
- Successful ride to work-day event.
- Snug River Track north upgrade.

8. KBAC new members onboarding

Suggestion that new members of the Committee should have an opportunity for an onboarding session. Anthony and Amanda to organise an onboarding meeting for new members in January.

NEXT MEETING

The next meeting of the Committee is to be confirmed.

CLOSURE

There being no further business, the Chairperson declared the meeting closed at 9:55am

Public Copy

Kingborough Bicycle Advisory Committee

Proposed Meeting Dates for 2025

*Note meetings are held bi-monthly on Fridays at 9:00am
in the Council Chambers, Kingston*

TBC

Public Copy

Appendix 1 – Cycling Strategy Action Tables:

Strategic and Advocacy Actions

Action	Project	Description	Time frame	Cost	Responsible Authorities	Funding	Progress/Outcomes
1	Ride to School Day and Ride to Work Day	Promote National Ride to School Day and Ride to Work Day through Council communication channels. Assess schools for rideability and barriers to greater cycling uptake.	Ongoing	Low	Council	Op	
2	Commuter Counts	Continue to collect cycling data by participating in the annual Commuter Counts in March each year.	Ongoing	Low	Council	Op	
3	Install usage counters	Install counters to collect usage data on cycling routes and create an active transport data set for the municipality.	Ongoing	Low	Council	Op	
4	Bike Week	Support Bike Week events to encourage participation in cycling, such as the Kingborough Treasure Hunt.	Ongoing	Low	Council	Op	
5	Road safety audits to eliminate hazards for people cycling	Review road crossings on shared paths and around schools to assess whether safety improvements can be made, such as wombat crossings and pedestrian refuges. Where kerb outstands create 'pinch points' identify options for removing the hazard. Review speed limits in activity areas and on cycling routes.	Ongoing	Low	Council	Op	
6	Active travel provisions in new developments	Advocate for the development of standards for active travel infrastructure and facilities. Encourage new developments and subdivisions to align with the recommendations of the <i>Kingborough Footpath Policy</i>	Ongoing	Low	Council, Tasmanian Planning Commission	Op	

Acti on	Project	Description	Time frame	Cost	Responsible Authorities	Fundi ng	Progress/Outcomes
		and the <i>Kingborough Cycling Strategy</i> endorsed by Council. Where appropriate require a 2.5m wide shared path alongside all collector roads and waterways in new developments. Encourage developers to incorporate bicycle parking in commercial and residential apartment developments.					
7	Promote safe cycling in Kingborough	Promote road safety campaigns developed by RSAC (Road Safety Advisory Council) and the Kingborough Community Safety Committee. Support and promote cycling education programs run by external agencies to provide bicycle education that increases skills and confidence.	Ongoing	Low	Council	Op	
8	Promote cycling routes and places to ride	Promote cycling-related tourism through provision of maps and online information about routes and destinations, such as Bruny Island.	Ongoing	Low	Council	Op	
9	Advocate for bike racks on buses	Advocate to MetroTas and the State Government to provide bike racks on buses on all local and regional bus routes.	Short	Low	Council, DSG	Op	
10	Wayfinding Strategy – signage and mapping	Develop and implement a Wayfinding Strategy, including improved signage and mapping for active travel throughout Kingborough.	Short	Low	Council	Op	
11	Positive Provisioning Policy	Adopt a Positive Provisioning Policy to incorporate cycling-friendly design in all Council projects.	Short	Low	Council	Op	

Infrastructure Actions

Action	Project	Description	Time frame	Responsible Authorities	Cost	Funding	Progress/Outcomes
12	Maintain existing bike network	Ensure the principal bicycle network is reviewed annually to ensure bicycle infrastructure is safe and surfaces are free of significant bumps, holes, or other impediments. Any issues should be included in road resurfacing or other road maintenance schedules.	Ongoing	Council, DSG	N/A	Op	
13	Bike parking	Install bike parking based on an annual audit and community surveying and at key bus stops.	Ongoing	Council, DSG	Low	Op	
14	Water stations and bike repair stations	Install water fountains and bike repair stations based on an annual audit and community surveying.	Ongoing	Council	Low	Op	
15	Channel Hwy sealed shoulders – Kingston to Kettering	Liaise with Department of State Growth to advocate for road upgrades along the Channel Highway that include 1.5m sealed shoulders.	Ongoing	State Growth, Council	Low	Op	
16	Mountain bike park improvements	Continue to seek opportunities and funding to upgrade the facilities at the Kingston Mountain Bike Park to bring it to contemporary standards.	Ongoing	Council	Low	Ex	
17	Channel Trail – Kingston to Margate	Investigate and advocate for a shared path from Huntingfield to the end of the existing shared path in Margate.	Short	DSG, Council	Low	Cap / Ex	Feasibility Study Report released
18	Algona Road	Investigate and advocate for a shared path on the southern side of Algona Road between Roslyn Avenue and Huntingfield.	Short	DSG, Council	Low	Cap / Ex	
19	Firthside to Summerleas	Construct a mixed on-road and shared path route between Kingston High School and the Firthside Park & Ride.	Short	Council, DSG	Medium	Cap / Ex	

Actio n	Project	Description	Time frame	Responsible Authorities	Cost	Fundi ng	Progress/Outcom es
20	Huntingfield Park & Ride Connections	Construct shared path and safe crossings from existing shared paths to the Huntingfield Park & Ride.	Short	Council, DSG	Mediu m	Cap / Ex	
21	Taroona – Channel Hwy	Upgrade on-road cycling facilities along Channel Highway through Taroona.	Short	Council, DSG	Mediu m	Cap / Ex	
22	Spring Farm to Sports Precinct	Construct a shared path from the existing Whitewater Creek path, north to the sporting precinct.	Short	Council	Mediu m	Cap / Ex	
23	Huntingfield to Kingston CBD – Channel Hwy	Advocate and seek funding for protected cycling infrastructure along Channel Highway, between Huntingfield and Kingston.	Short	DSG, Council	Mediu m	Ex	
24	Roslyn Avenue - Kingston Beach to Blackmans Bay	Investigate options for an uphill bicycle lane from Algona Road to Jindabyne Road. Investigate a shared path on the western side of Roslyn Avenue, including safe intersection upgrades.	Short	Council	Low	Cap	
25	Kingston to Kingston Beach	Investigate options and pursue opportunities for a shared path between Kingston and Kingston Beach.	Short	Council, Landowners	Low	Cap / Ex	
26	Channel Trail – Snug to Lower Snug	Construct a shared path between Snug and Lower Snug, starting from the existing shared path in Snug.	Short	Council, DSG	Very High	Cap / Ex	
27	Taroona Safe Route to School	Construct a mixed on-road and shared path route along Flinders Esplanade to Taroona Primary and High Schools, including modal filters and safe crossings.	Medium	Council, Education Dep.	Mediu m	Cap	
28	Sandfly Road – sealed shoulders	Improve on-road cycling infrastructure along Sandfly Road.	Medium	Council, DSG	High	Cap / Ex	Grant application being lodged for ‘safety lanes’ on the worst sections.

Actio n	Project	Description	Time frame	Responsible Authorities	Cost	Fundi ng	Progress/Outcom es
29	Blackmans Bay Beach Connections	Connect Tinderbox Road and Blowhole Road to Ocean Esplanade with cycling infrastructure.	Medium	Council	Medium	Cap	
30	Blackmans Bay Shops Connection	Provide a cut-through path from Roslyn Avenue and the Blackmans Bay Shops.	Medium	Council, Landowners	Low	Cap	
31	Roslyn Avenue – Algona Road to Illawarra	Investigate providing a shared path from Algona Road to Illawarra Primary School.	Medium	Council	Low	Cap	
32	Gormley Drive and Kingston View Drive	Link a shared path from Summerleas Road to the Sporting Precinct via the Twin Ovals.	Medium	Council	High	Cap	
33	Margate to Dru Point	Investigate a shared path link from Margate to Dru Point.	Medium	Council, Landowners	Low	Cap	
34	Margate local pathways	Investigate local access pathways and linkages for upgrades and slow street implementation in Margate.	Medium	Council	Low	Cap	
35	Snug local pathways	Investigate local access pathways and linkages for upgrades and slow street implementation in Snug.	Medium	Council	Low	Cap	
36	Redwood Road to Algona Road	Investigate a shared path from the Maranoa Heights Reserve paths to Algona Road.	Medium	Council, DSG	Low	Cap	
37	Channel Trail – Lower Snug to Kettering	Investigate a shared path between Lower Snug and Kettering.	Medium	DSG, Council	Low	Cap / Ex	
38	Lower Snug to Coningham	Construct a shared path between Lower Snug and Coningham.	Long	Council	High	Ex / Cap	
39	Redwood Road to Kingston CBD	Investigate a shared path from Lorikeet Drive to the Kingston CBD.	Long	Council	Low	Cap	
40	Longley to Neika – sealed shoulders	Improve on-road cycling infrastructure on Huon Road between Longley and Neika.	Long	Council	High	Cap / Ex	

Action	Project	Description	Time frame	Responsible Authorities	Cost	Funding	Progress/Outcomes
41	Harris Ct to Sherburd Ct path	Construct a path linking Harris Court to Sherburd Court.	Long	Council, Education Dep.	Medium	Cap / Ex	
42	Tingira Road to Ash Drive link	Investigate a shared path and/or on-road improvements, linking existing local tracks to Roslyn Avenue.	Long	Council	Low	Cap	
43	Ferry Road	Investigate improving active transport infrastructure along Ferry Road to Bruny Island Ferry Terminal.	Long	DSG, Council	Low	Ex	

Public Copy

E COMPLAINTS LODGED WITH COUNCIL 1 OCTOBER 2024 TO 31 DECEMBER 2024

File Number: File#
Author: Stephanie Velini, Business Improvement Officer
Authoriser: Dave Stewart, Chief Executive Officer

In accordance with Policy No. 1.20 Complaints Management Policy, the following summarises the complaints lodged with Council during the period 1 October 2024 to 31 December 2024. This information excludes complaints managed outside of this policy.

Complaints are analysed to identify trends and potential issues, for the purpose of improving administration and delivery of services relating to the complaints.

Service Type	
Compliance	6
Development Services	6
Projects	3
Roads & Stormwater	3
Waste Services	167
Works Department	8
Total	193

Issue Type - Category of complaint on lodgement	
Cost of services and fees	2
Council procedure / process	5
Delay in delivering a service	1
Delay in responding to a customer	8
Delay in taking an action	2
Lack of action taken	6
Lack of communication / consultation	3
Missed bin collections (approx. 330,000 collections/quarter)	167
Quality of action taken	4
Quality of decision made	1
Quality of interaction	1
Quality of service provided	5
Staff conduct - non serious	2

Investigation Type - How the complaint was dealt with	
Tier 1 - resolved at first point of contact (including missed bin collections)	182
Tier 2 - required further investigation	11
Tier 3 - internal review of the complaint decision requested	-
Tier 4 - external review of the complaint decision requested	-

Outcome of the complaint

Acknowledgement and/or apology provided	10
Change to policy or procedure	1
Complaint not substantiated	1
Complaint / investigation not yet finalised	3
Explanation of a decision or action or intention	16
Missed bin collections (approx. 330,000 collections/quarter)	167
Other	4
Request for service, not a complaint	1

Outcome of internal review

Original resolution was upheld	-
Original resolution was partially upheld	-
Original resolution was not upheld	-

Service Improvements - How the issue can be avoided in the future

Process, program or service review identified: Internal review and discussion regarding communication standards and response times (ongoing).

Process, program or service review identified: Review and discussion of the Summerleas Road Underpass Project to determine how it could have been better delivered (ongoing).

Process, program or service review identified: A continued focus on provision of good information and support through the assessment process (ongoing).

Service Delivery Compliments / Expressions of Appreciation 13

F COUNCILLOR ATTENDANCE AT MEETINGS AND WORKSHOPS 1 OCTOBER 2024 TO 31 DECEMBER 2024

The following table details Councillor attendances at Council meetings, S.23 Committees and Workshops during the year.

	Council		AGM		Audit Panel		Workshops		Leave of Absence Approved during the period
	Number Held	Number Attended	Number Held	Number Attended	Number Held	Number Attended	Number Held	Number Attended	
Mayor Cr Paula Wriedt	6	4	1	1			6	6	29/10/2024 - 7/11/2024; 2/12/2024 - 6/12/2024
Deputy Mayor Cr Clare Glade-Wright	6	4	1				6	6	4/10/2024 - 11/10/2024
Cr Aldo Antolli *	6	6	1	1	2	1	6	5	23/10/2024 - 27/10/2024; 2/1/2025 - 9/1/2025;
Cr David Bain *	6	5	1	1	2	2	6	6	18/11/2024 - 19/11/2024
Cr Gideon Cordover	6	6	1	1			6	6	
Cr Kaspar Deane	6	6	1	1			6	6	
Cr Flora Fox **	6	6	1	1	2	2	6	6	
Cr Amanda Midgley	6	5	1	1			6	6	4/11/2024
Cr Mark Richardson	6	5	1	1			6	6	28/9/2024 - 13/10/2024
Cr Christian Street	6	6	1	1			6	5	14/10/2024 - 18/10/2024

* Audit Panel Member

** Audit Panel Observer

Council Minute C390/14-12 determined that Councillor Attendance and Approved Leave of Absences be reported.

G COUNCILLOR ALLOWANCES & EXPENSES 1 JULY 2024 TO 31 DECEMBER 2024

Councillor	Councillor Allowances			Councillor Expenses						Total
	Mayor	Deputy Mayor	Councillors	Travel Allowances	Mayor's Vehicle	Bruny Ferry	Internet & Telephone	Conference & Meeting Attendance	Code of Conduct	
	\$	\$	\$	\$	\$	\$	\$	\$	\$	\$
Mayor Cr Wriedt	42,833		17,134	-	921	-	-	632		\$ 61,521
Deputy Mayor Glade-Wright		11,906	17,134	-	-	-	-	-		\$ 29,040
Cr Cordover			17,134	-	-	-	-	-		\$ 17,134
Cr Fox			17,134	515	-	-	470	825		\$ 18,944
Cr Midgley			17,134	-	-	-	-	798		\$ 17,932
Cr Street			17,134	-	-	-	-	-		\$ 17,134
Cr Antolli			17,134	-	-	-	-	-		\$ 17,134
Cr Richardson			17,134	-	-	-	-	-		\$ 17,134
Cr Deane			17,134	-	-	-	-	798		\$ 17,932
Cr Bain			17,134	-	-	-	-	798		\$ 17,932
		-	-	-	-	-	-	-		\$ -
Code of Conduct Complaints										\$ -
TOTAL	\$ 42,833	\$ 11,906	\$ 171,342	\$ 515	\$ 921	\$ -	\$ 470	\$ 3,850	\$ -	\$ 231,836

Notes:

Council Minute C390/14-12 determined that Councillor Allowances and Expenses paid under the "Payment of Councillors Expenses and Provision of Facilities" Policy be reported. Bruny Ferry costs are also included as required in Minute GF101/6-12 Bruny Ferry = cost of fares at \$38 per trip from 01/11/2018

H DONATIONS TABLE 1 JULY 2024 TO 31 DECEMBER 2024**MAYORAL DONATIONS**

Name	Description	Amount
Andrea Butchart	Mayor donation - Vic State Ice Skating Champs.	\$ 150.00
Emily Butchart	Mayor donation - Vic State Ice Skating Champs.	\$ 150.00
Illawarra Primary School	Jumping Jacks Team Contribution	\$ 250.00
Tassie Mums	Tassie Mums Event - Hub Hire 187/7/24	\$ 455.45
Salvation Army	Meal Program	\$ 250.00
MyCause (Lynna Taubman)	World Record Attempt	\$ 250.00
Kingston Park Run (Graeme Ingram)	Contribution to Park Run Birthday Celebrations	\$ 250.00
Dressed for Success	Internal Hub Hire 1/1 - 3/11/24	\$ 1,130.91
		\$ 2,886.36
	Annual Budget	\$ 4,000.00

COUNCIL POLICY DONATIONS

Name	Description	Amount
Eve Millar	Australian National Diving Championships	\$ 160.00
Oliver Wilson	Southern States Rugby Championships	\$ 150.00
Callum Degenaar	2024 Australian Orienteering Championships	\$ 160.00
Billy French	Coimbra Gymfest & Scalabis Cup Portugal	\$ 310.00
Amber French	Coimbra Gymfest & Scalabis Cup Portugal	\$ 310.00
Isaac Williams	Science & Engineering Challenge State Final	\$ 150.00
Niranjan Shibu	Science & Engineering Challenge State Final	\$ 150.00
Hayden De Kievit	Science & Engineering Challenge State Final	\$ 150.00
Grant Levitt	Southern States Rugby Championships	\$ 150.00
Judy Sekyere	Science & Engineering Challenge State Final	\$ 150.00
Hannah Joubert	Science & Engineering Challenge State Final	\$ 150.00
Pierre Macant	2024 Australian Orienteering Championships	\$ 150.00
Noah Joyce	School Sports Aust Hockey National Championships	\$ 160.00
Josie Smith	Science & Engineering Challenge State Final	\$ 160.00
Madeline Stanton	School Sports Aust Hockey National Championships	\$ 160.00
Olivia El-Tahche	Science & Engineering Challenge State Final	\$ 160.00
Aliera-Kate Carson	Science & Engineering Challenge State Final	\$ 160.00
Luca Rae Murgatroyd	Australian Football Championships	\$ 150.00
Samuel de Puit	Science & Engineering Challenge State Final	\$ 160.00
Laura Parsons	Science & Engineering Challenge State Final	\$ 160.00
Katie Clauson	2024 Australian Orienteering Championships	\$ 160.00
Hailee Richter	Science & Engineering Challenge State Final	\$ 160.00
Lily Smith	Science & Engineering Challenge State Final	\$ 160.00
Felix Woolley	Science & Engineering Challenge State Final	\$ 160.00
Emmett Bone	Science & Engineering Challenge State Final	\$ 160.00
Kaiya Wilkie	Science & Engineering Challenge State Final	\$ 160.00
Marni Duggan	Science & Engineering Challenge State Final	\$ 160.00
Abigail North	Science & Engineering Challenge State Final	\$ 160.00
Lachlan Michael	Science & Engineering Challenge State Final	\$ 160.00
Kathryn East	2024 AFL Masters National Carnival	\$ 160.00
Anne Smalley	2024 AFL Masters National Carnival	\$ 160.00
Izanne (Izzy) Viljoen	2024 AFL Masters National Carnival	\$ 160.00
Grace de Hoog	Science & Engineering Challenge State Final	\$ 160.00

Name	Description	Amount
Georgia Brouwer	Science & Engineering Challenge State Final	\$ 160.00
Christine Go	Science & Engineering Challenge State Final	\$ 160.00
Finn Wylie	National Youth Science Forum	\$ 160.00
Harry Finlayson	Science & Engineering Challenge State Final	\$ 160.00
Ryan Guy	Tas State Lawn Bowls Competition	\$ 160.00
Georgia Lange	U13 Girls State of Origin Championships	\$ 160.00
Charl Cilliers	Science & Engineering Challenge State Final	\$ 160.00
Margate Primary School	Twilight Fair Donation	\$ 200.00
Isabell Blaschke	2025 North Island (NZ) Colgate Games (Little Athletics)	\$ 310.00
Zoray Lim	Science & Engineering Challenge State Final	\$ 160.00
Ewan McIlwraith	Contribution to Tas Interstate Surf Champs	\$ 160.00
Blackmans Bay Primary School	School Citizenship Award 2024	\$ 100.00
Bruny Island District School	School Citizenship Award 2024	\$ 100.00
Calvin Christian School (Secondary)	School Citizenship Award 2024	\$ 100.00
Calvin Christian School (Primary)	School Citizenship Award 2024	\$ 100.00
Channel Christian School	School Citizenship Award 2024	\$ 100.00
Illawarra Primary School	School Citizenship Award 2024	\$ 100.00
Indie School	School Citizenship Award 2024	\$ 100.00
Kingston High School	School Citizenship Award 2024	\$ 100.00
Kingston Primary School	School Citizenship Award 2024	\$ 100.00
Margate Primary School	School Citizenship Award 2024	\$ 100.00
Snug Primary School	School Citizenship Award 2024	\$ 100.00
Southern Christian College	School Citizenship Award 2024	\$ 100.00
Taroona High School	School Citizenship Award 2024	\$ 100.00
Taroona Primary School	School Citizenship Award 2024	\$ 100.00
Woodbridge Primary School	School Citizenship Award 2024	\$ 100.00
Lewis Thorpe	All Schools Athletics	\$ 160.00
Jaxon Thorpe	All Schools Athletics	\$ 160.00
Tarremah Steiner School	School Citizenship Award 2024	\$ 100.00
Toby Webb	Underwater Hockey Championships	\$ 160.00
Daniel McKinley	Underwater Hockey Championships	\$ 160.00
Hugh Wass	Aust Primary Track and Field Championships	\$ 160.00
Arthur Whittock	Underwater Hockey Championships	\$ 160.00
Neve Hagan	Aust Track & Field Champs Contribution	\$ 160.00
Ashlin Hagan	Aust Track & Field Champs Contribution	\$ 160.00
Jasmine Taylor	Australian All Schools Athletics Championships	\$ 160.00
Jarrold Webb	Australian Underwater Hockey Championships	\$ 160.00
Tango Coull	Australian Underwater Hockey Championships 2025	\$ 160.00
		\$ 10,800.00
	Annual Budget	\$ 12,000.00

- Council makes Donations under section 77 of the Local Government Act 1993 which states that "Council may make a Grant for any purpose it considers appropriate ". "The details of any grant made ... are to be included in the annual report of the council " Section 77 (2).